



**HIGHWAY 401 IMPROVEMENTS
FROM HIGHWAY 410/403 INTERCHANGE TO
EAST OF THE CREDIT RIVER**

DETAIL DESIGN STUDY

CITY OF MISSISSAUGA, REGIONAL MUNICIPALITY OF PEEL

**CLASS ENVIRONMENTAL ASSESSMENT FOR PROVINCIAL
TRANSPORTATION FACILITIES**

GROUP 'B' PROJECT

MINISTRY OF TRANSPORTATION

CENTRAL REGION

GWP 2149-01-00 and 2150-01-00

TRANSPORTATION ENVIRONMENTAL STUDY REPORT ADDENDUM

June 2015

**TRANSPORTATION ENVIRONMENTAL STUDY REPORT ADDENDUM
HIGHWAY 401 IMPROVEMENTS DETAIL DESIGN STUDY**

**HIGHWAY 410/403 INTERCHANGE
TO EAST OF THE CREDIT RIVER
(GWP 2150-01-00)
CITY OF MISSISSAUGA
REGIONAL MUNICIPALITY OF PEEL**

**CLASS ENVIRONMENTAL ASSESSMENT
FOR PROVINCIAL TRANSPORTATION FACILITIES (GROUP 'B')**

Prepared and Reviewed by:



Mirjana Osojnicki, BES
Environmental Planner
AECOM



John Klowak, P.Eng.
Consultant Project Manager
AECOM

Reviewed by:



Larry Sarris
Environmental Planner
Ontario Ministry of Transportation

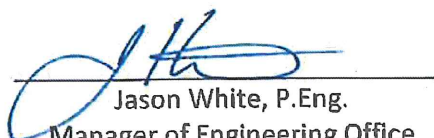


Mr. Paul Ahn, P. Eng.
Project Engineer
Ontario Ministry of Transportation

Approved by:



Linda Fischer
Head, Environmental Planning
Ontario Ministry of Transportation



Jason White, P.Eng.
Manager of Engineering Office
Ontario Ministry of Transportation

THE PUBLIC RECORD

A CD copy of this Addendum to the Transportation Environmental Study Report has been submitted to the following office of the Ministry of the Environment and Climate Change:

Ministry of the Environment and Climate Change

Central Region

5775 Yonge Street, 8th Floor
Toronto, ON M2M 4J1

A copy of this Addendum to the Transportation Environmental Study Report is available for review at the following locations:

Ministry of Transportation

Central Region

Planning & Environmental Office

1201 Wilson Avenue Building D,
3rd Floor, Downsview, ON M3M 1J8
Telephone: 416.235.5412

Monday-Friday: 8:30am to 4:30pm
Weekends: Closed

The City of Mississauga

Office of the City Clerk

300 City Centre Drive, 2nd Floor
Mississauga, ON L5B 3C1
Telephone: 905.615.4311

Monday-Friday: 8:30am to 4:30pm
Weekends: Closed

Courtneypark Branch Library

730 Courtneypark Drive West
Mississauga, ON L5W 1L9
Telephone: 905.615.4745

Monday-Friday: 8:00am to 9:00pm
Saturday: 9:00 am to 5:00 pm
Sunday: 1:00 pm to 5:00 pm

Region of Peel

Clerk's Office

10 Peel Centre Drive
Brampton, ON L6T 4B9
Telephone: 905.791.7800

Monday-Friday: 8:00am to 5:00pm
Weekends: Closed

Ce document hautement specialise n'est disponible qu'en anglais en vertu du reglement 411/97, qui en exempte l'application de la Loi sur les services en français. Pour de l'aide en français, veuillez communiquer avec le ministere des Transports, Bureau des services en français au: 905-704-2045 ou 905-704-2046.

Executive Summary

In 2011, the Ministry of Transportation (MTO) retained AECOM Canada Limited (AECOM) to carry out the Detail Design for the ultimate widening of Highway 401 from the Highway 403/410 interchange to the Credit River, a distance of 7 km under GWP 2150-01-00. The project will complete the expansion of Highway 401 within the project limits from its current 6 lanes to a 12-lane core/collector system, as documented in the Transportation Environmental Study Report (TESR) for the *Preliminary Design of Highway 401 From Highway 410/403 Interchange to East of the Credit River (GWP 2149-01-00 and GWP 2150-01-00)*, August 2005. The TESR was approved in 2007 with Minister's Conditions.

The City of Mississauga requested the MTO to study the need for a future extension of Belgrave Road from the existing cul-de-sac west of Suffolk Court westerly to the Highway 401 eastbound off ramp at Mavis Road – a distance of approximately 200 m. The extension will provide a future direct connection to Mavis Road, thereby improving traffic service to commercial and industrial areas to the east. This Transportation Study Report Addendum confirms the need to extend Belgrave Road and documents the design modification to the interchange ramps at Mavis Road to accommodate both the Highway 401 widening and the extension of Belgrave Road.

The 2010 Hurontario/Main Street Master Plan identified Hurontario Street – Main Street as an important transportation corridor within the Cities of Mississauga and Brampton. The Hurontario - Main Light Rapid Transit (LRT) will reduce the number of traffic lanes along this north-south corridor. Improvements to the surrounding road system in this area will facilitate improved vehicular traffic flow once the LRT is in operation. At the request of the City of Mississauga, AECOM completed a traffic diversion analysis for Mavis Road, for the preferred alternative connection to Belgrave Road from the Highway 401 eastbound off-ramp at Mavis Road. The analysis supports the need for improving traffic service to the commercial/industrial areas to the east once the Hurontario – Main Light Rapid Transit is constructed.

Alternative B - At Grade Intersection with Mavis Road Structure over Realigned Ramp S-E was selected to be carried forward as the Recommended Plan. It includes an at-grade connection that would provide 2-way access to Belgrave Road from Mavis Road. Some turning movements will be restricted at the intersection (i.e., Belgrave Road, westbound) to optimize traffic operations. In addition, a south to east at-grade ramp will be constructed under the Belgrave Road extension.

The MTO will construct the south-east ramp on a lower profile and the City of Mississauga will construct a bridge over the ramp to connect Belgrave Road at a later date.

This alternative has been selected due to:

- Operational benefits including providing free flow of traffic and provision of a 'right out' access to northbound Mavis Road
- Allows for the protection of a future Belgrave Road extension within the current MTO contract.

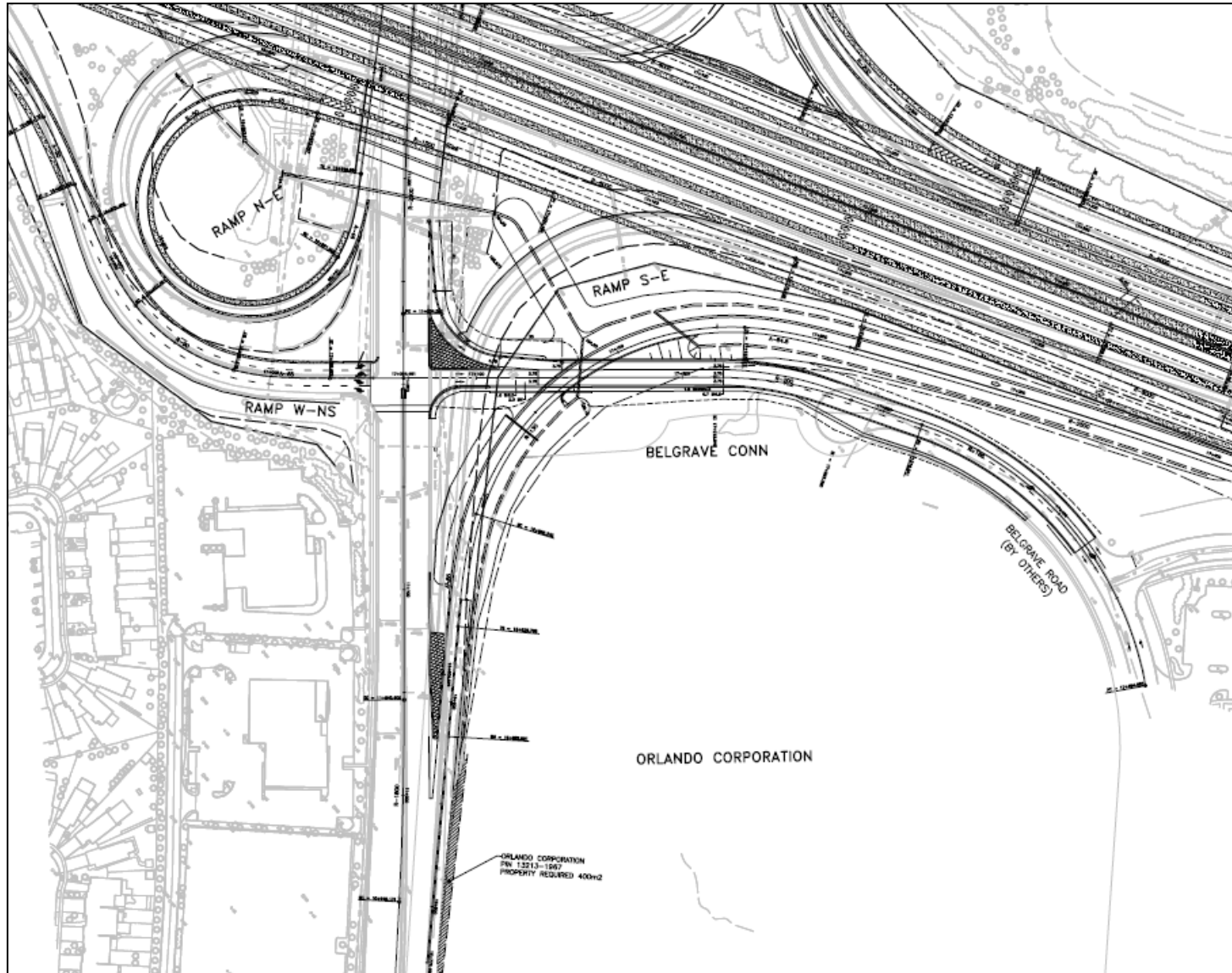
This alternative accommodates a section which comprises of two (2) eastbound lanes and one (1) westbound lane each at a 3.75 m width, with one of the eastbound lanes becoming a left turn lane at the next intersection. Due to the at-grade nature of the design, a sidewalk has been included to allow pedestrian access from Mavis Road to Belgrave Road. Please refer to **ES1. Recommended Plan**.

As part of the overall Detail Design assignment, a consultation plan was prepared in recognition of the integral role that stakeholders play in the project process. The stakeholder groups include the following:

- General public, including adjacent property owners
- Federal and provincial government agencies (including conservation authorities)
- Municipal staff and councils
- Utility companies.

In keeping with the MTO protocol, First Nations were also included as part of the consultation.

ES1. Recommended Plan - At Grade Intersection with Mavis Road Structure over Realigned Southeast Ramp



Information about the following environmental conditions was collected in order to compare alternatives for the highway widening and to assess the environmental impacts of the selected design:

- Natural Environment
- Socio-Economic Environment
- Cultural Environment
- Traffic.

Table **ES2.** below provides a summary of the anticipated environmental concerns and mitigation measures associated with this project.

ES2. Summary of Environmental Protection / Mitigation Requirements Prior to and During Construction

I.D.#	Issues/Concerns Potential Effects	Concerned Agencies	I.D.#	Mitigation/Protection/Monitoring
General Environmental Protection Requirements				
1	General Environmental Protection	MTO	1.1	<ul style="list-style-type: none"> The general requirement to protect the natural environment from impacts and damage, including physical damage, emissions, and local access; and Control of materials, equipment, construction method, construction disturbance limits, and work timing/staging duration, in order to achieve the foregoing.
Natural Environment Concerns and Commitments				
2	Fisheries and Watercourse Impacts	MNRF	2.1	<ul style="list-style-type: none"> No watercourse within study area; and No mitigation required.
3	Wildlife Impacts (low probability)	MNRF	3.1	<ul style="list-style-type: none"> Temporary vegetation protection fencing installed at the edge of the clearing / grubbing limits; Requirement that construction equipment and vehicles give a right-of-way to wildlife, allowing wildlife to pass and proceed to a safe distance prior to construction equipment/vehicles commencing construction activities; and Requirement that work cease and Contract Administrator be notified in the event wildlife is injured during construction activities.
4	Bird Impacts (in long grass)	MNRF	4.1	<ul style="list-style-type: none"> Vegetation and site clearing will take place between September and March 31st and avoid the months of April until August, in accordance with the <i>Migratory Birds Convention Act</i>.
5	Potential to encounter Species at Risk during construction	MTO/MNRF	5.1	<ul style="list-style-type: none"> If Species at Risk is encountered a qualified biologist shall be contacted in order to determine how to proceed without contravening the ESA (2007).
6	Vegetation impacts	MNRF	6.1	<ul style="list-style-type: none"> Vegetation removal will be kept to a minimum and exposed soils will be re-vegetated with an approved MTO seed mix that suits the area soils, immediately following the completion of required work and the final grade has been established.
7	Erosion and sediment control	MTO	7.1	<ul style="list-style-type: none"> An erosion and sediment control plan for construction shall be developed and effectively implemented. It will include: <ul style="list-style-type: none"> Limiting vegetation removal to only what is required for grading and ditching operations; Constructing embankment slopes at a maximum ratio of 2:1; and Establishing cover as soon as possible after excavation. Sediment fences around construction activities.

I.D.#	Issues/Concerns Potential Effects	Concerned Agencies	I.D.#	Mitigation/Protection/Monitoring
Socio-Economic Environment Concerns and Commitment				
8	Construction Noise	MTO/MOECC	8.1	<ul style="list-style-type: none"> Idling of construction equipment will be kept to a minimum and maintained in good working order; and Contract documents provided to the contractor will contain general noise control measures to mitigate the noise impact at noise sensitive areas including two standard clauses regarding equipment noise.
			8.2	<ul style="list-style-type: none"> Respond to construction noise complaints according to MTO/MOE "Noise Protocol".
9	Dust Control during Construction	MTO/MOECC	9.1	<ul style="list-style-type: none"> A dust control plan for construction shall be developed and effectively implemented.
10	Spill Prevention and Response	MTO	10.1	<ul style="list-style-type: none"> Contractor shall have a spill prevention and response contingency plan that addresses procedures for preventing and responding to spills, and the equipment (including a spill response kit) and resources that will be available.
			10.2	<ul style="list-style-type: none"> Refuelling precautions will be taken; and Refuelling will be conducted only in designated areas preferably situated on an impermeable surface.
			10.3	<ul style="list-style-type: none"> Construction equipment required to be in good condition.
11	Management of Surplus Material and Debris	MTO	11.1	<ul style="list-style-type: none"> Management materials will be dealt with in accordance with normal MTO practices (regulated by OPSS 180).
			11.2	<ul style="list-style-type: none"> A Phase II ESA will be completed during Detail Design prior to construction tendering.
12	Potential for temporary or permanent change to emergency access	OPP/Peel Region	12.1	<ul style="list-style-type: none"> MTO will notify OPP and emergency medical services of construction staging, start of construction etc. to minimize delay in emergency response times during and after construction.
13	Potential impacts to existing utilities	Rogers Cable Bell Canada Enersource Hydro	13.1	<ul style="list-style-type: none"> Utility relocation will be coordinated prior to construction.
Cultural Environment Concerns and Commitments				
14	Impacts to archaeological resources	MTCS	14.1	<ul style="list-style-type: none"> A Stage 2 Archaeological Assessment will be completed by the City of Mississauga during detail design so that archaeological clearance can be obtained for the extension prior to tendering.
			14.2	<ul style="list-style-type: none"> During construction, if any archaeological and/or historical resources are discovered, they may be a new archaeological site and therefore subject to Section 48(1) of the <i>Ontario Heritage Act</i>. Work in the area would not resume until cleared by the Ministry of Tourism, Culture and Sport.
15	Impact to cultural heritage landscape (Highway 401 Roadscape)	MTCS	15.1	<ul style="list-style-type: none"> No mitigation required.

Table of Contents

MTO Signature Page
Public Record
Executive Summary

	page
1. Overview of the Project	1
1.1 Project Description and Background	1
1.2 Environmental Assessment Addendum Report Process.....	2
1.3 Canadian Environmental Assessment Act 2012	3
1.4 Environmental Protection Requirements	3
1.4.1 Endangered Species Act 2007	3
1.4.2 Migratory Birds Convention Act, 1994	4
1.5 Study Process.....	4
1.6 Transportation Engineering and Environmental Protection Principles and Processes	7
2. Consultation	8
2.1 Consultation Plan.....	8
2.2 Project Contact	8
2.3 Notice of Commencement of Detail Design.....	8
2.4 Project Website.....	9
2.5 Consultation with External Agencies	9
2.6 Consultation with First Nations	10
2.7 Consultation with Utility Companies	11
2.8 Public Information Centre (PIC).....	11
2.8.1 Notification of the PIC	11
2.8.2 PIC Organization and Logistics	11
2.8.3 PIC Attendance.....	12
2.8.4 Summary of PIC Participant Feedback	12
2.8.4.1 Comments Received	13
2.9 Filing of the TESR Addendum	13
3. Existing Environmental Conditions and Changes.....	14
4. Proposed TESR Design Changes	15
4.1 Belgrave Road Connection Alternatives.....	15
4.1.1 Alternative A – Mavis Road Underpass to Belgrave Road.....	15
4.1.2 Alternative B – At Grade Intersection with Mavis Road Structure over Realigned Ramp S-E	16
4.1.3 Alternative C – At-Grade Intersection with Mavis Road Infrastructure over Realigned Ramp S-E	17
4.1.4 Recommended Alternative	18
5. Environmental Concerns and Proposed Mitigation.....	20
5.1 Natural Environment.....	20
5.1.1 Fisheries and Aquatic Habitat.....	20
5.1.2 Wildlife	20
5.1.3 Vegetation.....	20
5.1.4 Wetlands.....	20

5.1.5	Drainage	21
5.1.6	Erosion and Sediment Control.....	21
5.2	Socio-Economic Environment	21
5.2.1	Noise.....	21
5.2.2	Property Waste and Contamination.....	21
5.2.3	Air Quality	22
5.2.4	Property	22
5.2.5	Utility	22
5.3	Cultural Environment	22
5.3.1	Archaeological Resources.....	22
5.3.2	Built and Cultural Heritage Resources	22
5.4	Traffic.....	22
5.5	Summary of Environmental Protection / Mitigation Requirements Prior to and During Construction.....	23
6.	Environmental Monitoring / Inspection Program for Project Implementation	26
6.1	Overview of Environmental Monitoring / Inspection Program for Project Implementation	26
6.2	Environmental Monitoring / Inspection During Construction	26
6.3	Post-Construction Environmental Monitoring / Inspection.....	27

List of Figures

Figure 1.	Proposed Belgrave Road Extension Study Area	2
Figure 2.	2005 TESR Addendum/Detail Design Key Steps	6
Figure 3.	Belgrave Road Alternative A – Plan View	15
Figure 4.	Belgrave Road Alternative B – Plan View	16
Figure 5.	Belgrave Road Alternative C – Plan View.....	17

List of Tables

Table 1.	Agency Correspondence from Public Information Centre Notification.....	13
Table 2.	Summary of Key Advantages/Disadvantages of Alternatives A, B and C	18
Table 3.	Summary of Environmental Protection / Mitigation Requirements Prior to and During Construction	24

Appendices

- Appendix A. Consultation
- Appendix B. Recommended Plan

1. Overview of the Project

1.1 Project Description and Background

In 2011, the Ministry of Transportation (the Ministry, or MTO) retained AECOM Canada Limited (AECOM) to carry out the Detail Design for the widening of Highway 401 from the Highway 403/410 interchange to the Credit River, a distance of 7 km under GWP 2150-01-00. The project will complete the expansion of Highway 401 within the project limits from its current 6 lanes to a 12-lane core/collector system, as documented in the Transportation Environmental Study Report (TESR) for the *Preliminary Design of Highway 401 From Highway 410/403 Interchange to East of the Credit River (GWP 2149-01-00 and GWP 2150-01-00)*, August 2005. The TESR was subsequently approved in 2007 with Minister's Conditions.

The proposed improvements are required to address deteriorating operating conditions, create opportunities for high occupancy vehicle infrastructure and incorporate existing and future traffic demands. The project will assist in facilitating the efficient and safe movement of goods and people to ensure the economic vitality of communities along the highway and throughout the province.

The 2005 TESR documented the:

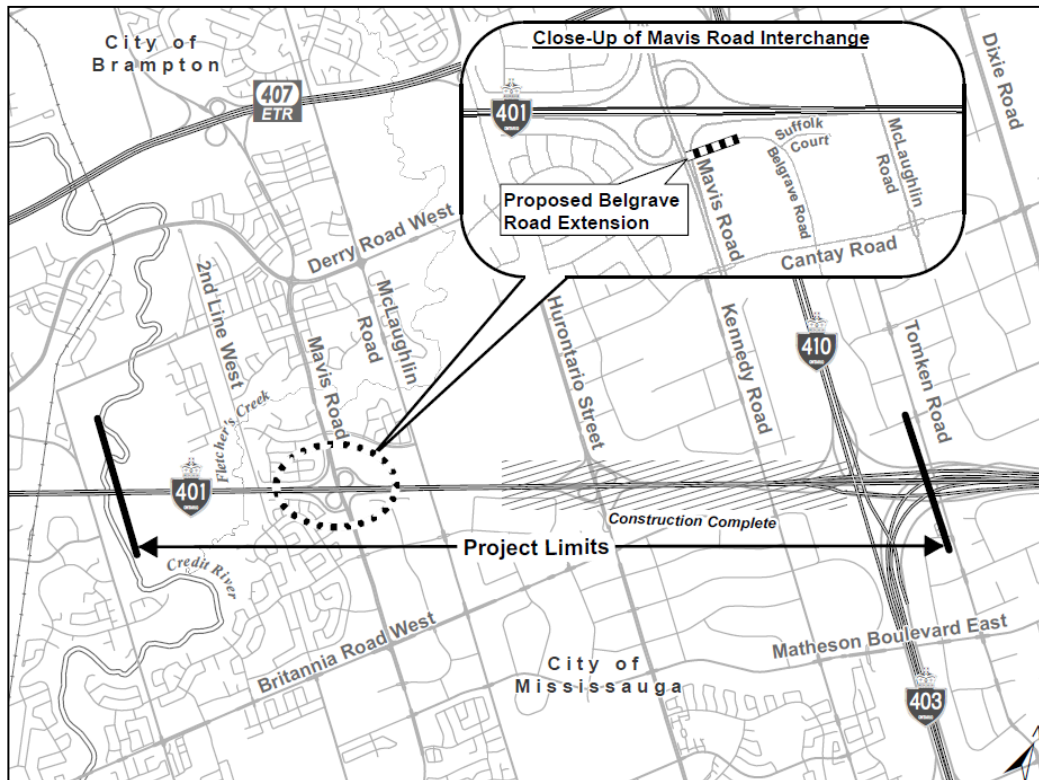
- Description of the project and its purpose
- Existing natural, socio-economic, cultural environment factors along with existing transportation and utilities
- Potential environmental effects and proposed mitigation measures
- Commitments to further work, consultation and monitoring associated with Detail Design and implementation of the project.

The recommended design in the 2005 TESR included:

- Widening of Highway 401 from a 6-lane highway to a 12-lane express/collector configuration
- Collector lanes consisting of two (2) general purpose lanes and one (1) HOV lane in each direction
- Reconstruction of the Hurontario Street interchange, including expansion of the existing structure over Highway 401
- Reconstruction of the Mavis Road interchange, including expansion of the existing structure over Highway 401;
- Replacement of the McLaughlin Road structure over Highway 401
- Removal of the 2nd Line West structure over Highway 401.

The Detail Design stage of this project commenced in June 2011.

At the request of the City of Mississauga, the Ministry investigated the need for a future extension of Belgrave Road from the existing cul-de-sac west of Suffolk Court westerly to the Highway 401 eastbound off ramp at Mavis Road (i.e., a distance of approximately 200 m), as shown in Figure 1. The extension will provide a future direct connection to Mavis Road, thereby improving traffic service to commercial and industrial areas to the east. This TESR Addendum confirms the need to extend Belgrave Road and documents the design modification to the interchange ramps at Mavis Road to accommodate both the Highway 401 widening and the extension of Belgrave Road. The alternatives developed for the Belgrave Road connection and the recommended alternative are outlined in Section 4.0 of this Report. Consultation with local residents and municipalities to review and discuss the crossing alternatives is outlined in Section 2.0.

Figure 1. Proposed Belgrave Road Extension Study Area

1.2 Environmental Assessment Addendum Report Process

In accordance with Section 6.1 and 6.4.1 of the MTO's *Class Environmental Assessment for Provincial Transportation Facilities* (2000), an Addendum is necessary if there are significant changes to the commitments outlined in the TESR or changes to the concept of portions of the project.

This TESR Addendum is intended to document the rationale and design modification for the addition of the Belgrave Road connection to provide a future direct connection to Mavis Road, thereby improving traffic service to commercial and industrial areas to the east. The key changes to the design of Highway 401 interchange at Mavis Road documented in the 2005 TESR are the following:

- An at-grade connection allowing 2-way access to Belgrave Road from Mavis Road will be provided by forming a 4-leg intersection at the existing intersection of the Highway 401 eastbound off-ramp at Mavis Road. Some turning movements will be restricted at the intersection (i.e. Belgrave Road westbound Left) to optimize traffic operations.
- The south to east at-grade ramp will be constructed at a lower profile under the Belgrave Road extension and a bridge will be constructed over the ramp to connect Belgrave Road.
- The Belgrave Road extension will be comprised of two (2) eastbound lanes and one (1) westbound lane, with one of the eastbound lanes becoming a left turn lane at the next intersection. A sidewalk will be provided to allow pedestrian access from Mavis Road to Belgrave Road.
- MTO will construct the ramp and at a later date the City of Mississauga will construct the bridge and the extension to Belgrave Road.

The TESR Addendum is being made available for a 30-day public and agency review period and has been provided to the affected and interested parties and agencies. Furthermore, the Addendum has been posted on the Highway 401 project website at www.401expansion-mississauga.ca, for electronic viewing.

Interested persons are encouraged to review the TESR Addendum and provide written comments to one of the following Project Team members during the 30-day review period (**ending July 10, 2015**).

John Klowak, P. Eng.
Consultant Project Manager
AECOM Canada Limited
30 Leek Crescent
Richmond Hill, ON, L4B 4NK
Phone: 905-882-4401
Fax: 905-882-4399
E-mail: John.Klowak@aecom.com

Mr. Paul Ahn, P.Eng.
MTO Project Engineer
Highway Engineering, Peel & Halton Section
4th Floor
1201 Wilson Avenue
Downsview, ON, M3M 1J8
Phone: 416-235-4949
Fax: 416-235-3576
Email: Paul.Ahn@ontario.ca

If, after consulting with MTO and Consultant staff, you still have serious unresolved concerns, you have the right to request the Minister of the Environment (in writing to: **77 Wellesley Street West, 11th Floor, Ferguson Block, Toronto, Ontario M7A 2T5**) to issue a Part II Order (i.e., “bump up”) for the proposed changes to the improvements to this section of Highway 401. A Part II Order may lead to the preparation of an Individual Environmental Assessment. A copy of the Part II Order request that is sent directly to the Minister of the Environment and Climate Change should also be forwarded to Ministry of Transportation and Consultant staff at the addresses listed above. If there are no outstanding concerns at the end of the 30-day review period, the project will be considered to have met the requirements of the *Class EA*.

Only the changes documented in the TESR Addendum Report are eligible for bump-up. In the event that a Part II Order request is granted, MTO has the option to withdraw the Addendum.

The decision whether a Part II Order is appropriate or necessary rests with the Ministry of the Environment and Climate Change.

If no Part II Order requests are received by the end of the 30-day review period, the project is considered to have met the requirements of the Class EA and the project may continue to the Detail Design and construction phase subject to obtaining any other outstanding environmental approvals.

1.3 Canadian Environmental Assessment Act 2012

The Canadian Environmental Assessment Act (CEAA) was repealed and replaced with CEAA 2012 which received Royal Assent on July 6, 2012. Changes to CEAA include replacing “triggers” with the CEAA 2012 Regulation Designating Physical Activities list. A proponent is not required to complete the federal EA Process if a project is not on this list. It has been determined that the Highway 401 Improvements project, including the proposed Belgrave Road connection does not include physical activities identified on the list and is therefore not a Designated Project. Therefore this project is not subject to the new federal EA process.

1.4 Environmental Protection Requirements

1.4.1 Endangered Species Act 2007

Endangered is defined as a species facing imminent extinction or extirpation in Ontario.

Subsection 9(1) of the *Endangered Species Act, 2007* (ESA) states in part that:

No person shall,

(a) Kill, harm, harass, capture or take a living member of a species that is listed on the Species at Risk in Ontario List as an extirpated, endangered or threatened species.

Clause 10(1)(a) of the ESA states that:

No person shall damage or destroy the habitat of a species that is listed on the Species at Risk in Ontario list as an endangered or threatened species.

Provided the applicable legislated requirements in subsection 17(2) of the ESA are met, the Minister may issue a permit to a person under subsection 17(1) of the Act that authorizes the person to engage in an activity that would otherwise be prohibited by subsection 9(1) or 10(1) of the Act. Of relevance to the proposed Highway 401 undertaking is the 17(2)(c) permit. This permit may be issued for authorizing activities where the activity:

Has the main purpose not to assist in the protection or recovery of the species, but through specific and mandatory conditions outlined in the permit will result in an overall benefit to the species within a reasonable time.

As discussed in Section 5 of this Report, field investigations in 2011 and 2012 confirmed that there are no Species at Risk within the Highway 401/Mavis Road interchange. Therefore, no permits under the *Endangered Species Act, 2007* will be required. In addition, previous discussions with the Ministry of Natural Resources and Forestry have confirmed that the proposed works do not require approval under the *Act*.

1.4.2 Migratory Birds Convention Act, 1994

The federal *Migratory Birds Convention Act* is applied through:

The Regulations Respecting the Protection of Migratory Birds that states that “[...] no person shall disturb, destroy or take a nest, egg [...] of a migratory bird.” This law protects all birds aside from the introduced species European Starling (*Sturnis vulgaris*), House Sparrow (*Passer domesticus*), and Rock Pigeon (*Columba livia*). Bird nests that are destroyed during the course of construction and other related activities is referred to as “incidental take” and is illegal except under the authority of a permit obtained through the Canadian Wildlife Service (CWS).

Requirements under the *Migratory Birds Convention Act* would apply to this project at the time of pre-construction vegetation clearing and during construction, should nests be found. No vegetation clearing will be permitted during the nesting period. Generally, the period during which vegetation clearing is prohibited is between the end of March to mid-august.

It should be noted that field reconnaissance activities in 2011 and 2012 did not observe any active nests within the Highway 401/Mavis Road interchange.

1.5 Study Process

The key steps in the TESR Addendum/Detail Design are highlighted in Figure 2.

Detail Design and preparation of the contract documentation will continue after the TESR Addendum has been filed and receives Environmental Clearance. There is a possibility that the final design plans may identify design modifications or refinements which could result in environmental benefits or impacts that may not have been anticipated or identified in this document. However, should this occur, the modifications are not anticipated to alter the basic intent of the undertaking and any anticipated environmental impacts will be addressed.

Upon completion of the Highway 401 Improvements Detail Design, a Design and Construction Report (DCR) will be prepared. The DCR will document the updated Detail Design plan and the proposed environmental mitigation measures.

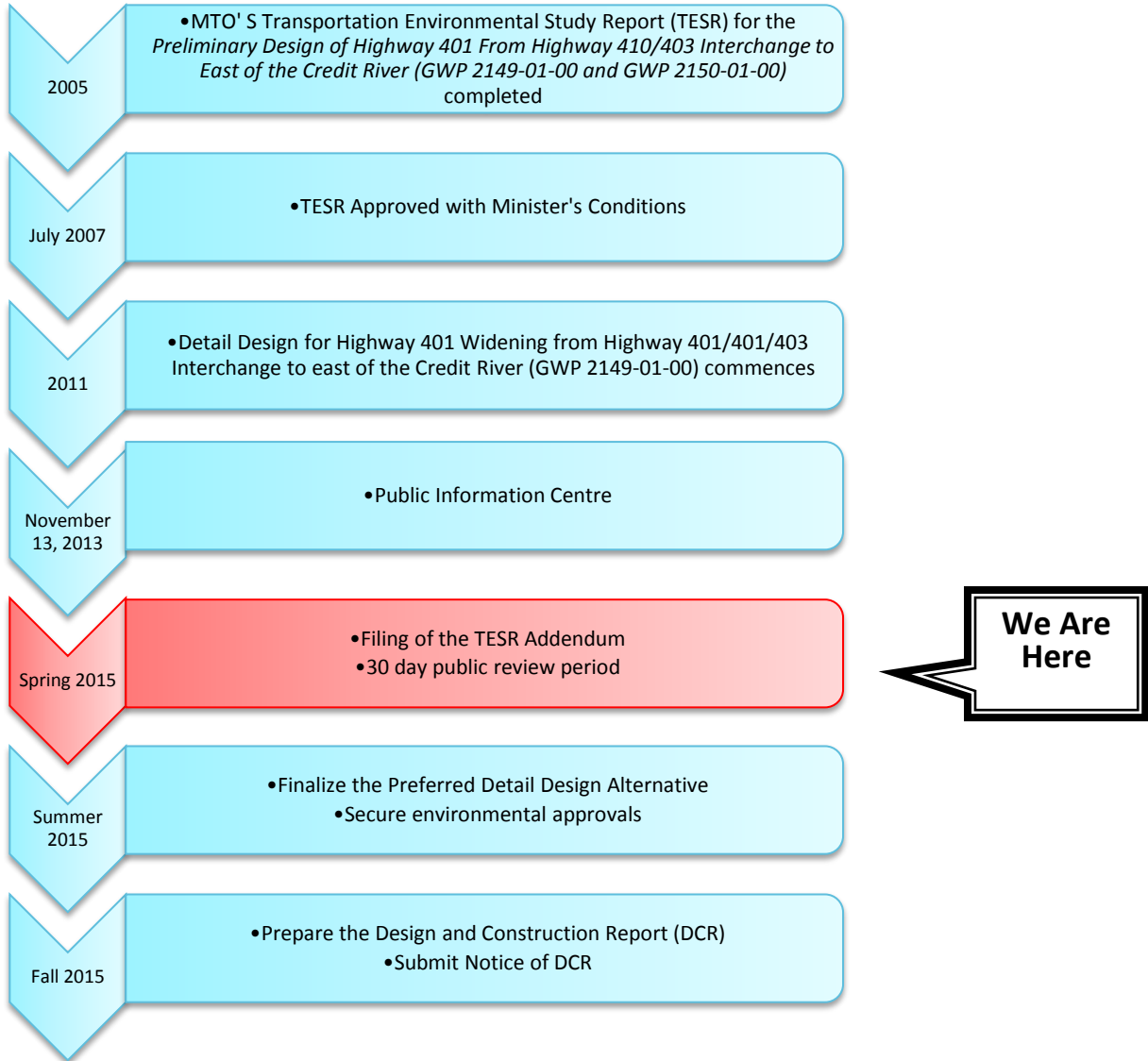


Figure 2. 2005 TESR Addendum/Detail Design Key Steps

1.6 Transportation Engineering and Environmental Protection Principles and Processes

The Ministry of Transportation has developed a number of transportation engineering, planning, and design principles that underlie the Class EA process for all Group A, B, and C projects. The principles were developed by the MTO in order to achieve a planning and design process that is principle-based rather than prescriptive. This principle-based approach was applied during the facility design and decision-making process for this project. The principles outlined in the Class EA are:

- Transportation Engineering Principles
- Environmental Protection Principles
- External Consultation Principles
- Evaluation Principles
- Documentation Principles
- Bump-Up Principles
- Environmental Clearance Principles to Proceed.

The study principles listed above were applied to this project as set out in the Class EA document.

2. Consultation

2.1 Consultation Plan

As part of the overall Detail Design assignment, the Project Team consulted with federal and provincial ministries and agencies, elected officials, municipalities, emergency services providers, utilities, First Nations and interest groups at key milestones.

A consultation plan was prepared by the Project Team and designed to fulfill the following objectives:

- Ensure that the stakeholder groups identified above and First Nations have an opportunity to participate in the project process, as well as contribute to decisions at an appropriate time
- Ensure that factual information was provided to stakeholders and First Nations, as soon as reasonably possible
- Make contact with government agencies to obtain background technical information and policy advice
- Make contact with owners of adjacent property that may be impacted by the project recommendations.

The details of the consultation plan were consistent with the consultation requirements of MTO's *Class Environmental Assessment for Provincial Transportation Facilities* (2000) and *Environmental Reference for Highway Design* (2013) and are provided in the following subsections.

Participant input was incorporated into the project findings and recommendations, as appropriate and written responses were provided to all written stakeholder input (letters, emails, Public Information Centres (PICs), comment sheets).

All project correspondence to/from the public was collected in accordance with the *Freedom of Information and Protection of Privacy Act*. Accordingly, with the exception of person contact information, all public comments are part of the public record.

2.2 Project Contact

A project contact/ mailing list was developed and continually updated to support mailing out of letters at the following points of the study:

- Notice of Commencement
- Notice of Public Information Centre (PIC)
- Notice of TESR Addendum filing.

The project contact/ mailing list initially included First Nations, federal government agencies, area Members of Provincial Parliament (MPP), Members of Parliament (MP), provincial government agencies (including conservation authorities), municipal staff and politicians, the impacted property owners and members of the public who had requested to be kept on the mailing list from the 2005 TESR. The project mailing list was also updated from comments received following the issuance of the Notice of Commencement, Notice of PIC and those received through the project website. The number of contacts on this list increased as the overall project proceeded.

2.3 Notice of Commencement of Detail Design

The Notice of Commencement of Detail Design was placed in the following newspapers:

- Toronto Star (English) – Tuesday, June 21, 2011
- Mississauga News (English) – Wednesday, June 22, 2011
- L'Express de Toronto (French) – Tuesday, June 28, 2011.

The Notice provided details regarding the process to be followed, solicited input from interested and affected parties, requested members of the public to identify themselves if they wished to be added to the project mailing lists and provided the address, telephone number and email address of both the MTO and AECOM Project Managers.

External agencies (including government agencies, municipalities) and utility companies were first notified of the start of Detail Design electronically, via e-mail on June 21, 2011 due to a nation-wide Canada Post strike. Once Canada Post resumed mailing operations, a hard-copy notification, which included a study area map and External Agency Comment Sheet, was mailed on June 27, 2011. Input was requested by July 25, 2011. First Nations were notified of project commencement on June 20, 2011.

Copies of the notice, emails and letters are provided in **Appendix A**.

2.4 Project Website

A website for the overall project was developed to provide background information and project updates: www.401expansion-mississauga.ca. Visitors to the website were also given an opportunity to submit comments directly to the MTO Project Manager.

2.5 Consultation with External Agencies

The federal, provincial and municipal governments, along with MPs and MPPs that were contacted during this project include the following:

Federal Agencies

- Canadian Environmental Assessment Agency
- Fisheries and Oceans Canada
- Environment Canada
- Aboriginal Affairs and Northern Development Canada
- Transport Canada
- Canadian Transportation Agency

Municipal Agencies

- Region of Peel
- Peel Region Police
- City of Mississauga
- City of Brampton
- Peel District School Board
- Dufferin-Peel Catholic District School Board
- TransHelp
- Mississauga Board of Trade

Provincial Agencies

- Ministry of Aboriginal Affairs
- Ministry of Agriculture and Food
- Ministry of Energy and Infrastructure
- Ministry of Environment and Climate Change
- Ministry of Natural Resources and Forestry
- Ministry of Municipal Affairs and Housing
- Ministry of Tourism, Culture and Sport
- Credit Valley Conservation Authority
- Toronto Region Conservation Authority
- Ontario Provincial Police
- GO Transit and Metrolinx

MP & MPPs

- MPP, Mississauga-Streetsville
- MP, Mississauga-Streetsville
- MPP, Mississauga-Brampton South
- MP, Mississauga-Brampton South

Consultation with government agencies facilitated identification of key issues/concerns and working to cooperatively develop mutually agreeable planning solutions/strategies in a timely manner.

As the project progressed, government agencies were provided with notification of the PIC and notification of the filing of the TESR Addendum. Follow-up was undertaken during the project as appropriate, with respect to issues and concerns that were identified.

A summary of specific issues raised (including reference to the respective agency) during the project is provided below.

City of Mississauga

Meetings were held with the City of Mississauga to discuss opportunities for the City and MTO to work together and identify issues and solutions to benefit both parties. Items of discussion directly related to this project included traffic staging during reconstruction of the Mavis Road interchange and a future extension of Belgrave Road from the existing cul-de-sac west of Suffolk Court westerly to the Highway 401 eastbound off ramp at Mavis Road. Meeting minutes are provided in **Appendix A**. These discussions will continue into Detail Design for the overall project.

Ministry of Natural Resources and Forestry

Meetings with the Ministry of Natural Resources and Forestry (MNR) have taken place to identify and discuss potential impacts of the Highway 401 widening on sensitive species, as well as the proposed stormwater management strategy for the overall project. No issues were identified for the Belgrave Road extension (Mavis Road/Highway 401 interchange) area, as evidence of species-at-risk was not present. Discussions with the MNR will continue into Detail Design for the overall project. Meeting minutes are provided in **Appendix A**.

Credit Valley Conservation Authority

A meeting was held with the Credit Valley Conservation Authority, together with the MNR and the City of Mississauga on November 25, 2011 (as described above). Meeting minutes are provided in **Appendix A**.

2.6 Consultation with First Nations

The First Nations and First Nations agencies contacted as part of the project, included:

- Union of Ontario Indians, Nipissing First Nation
- Association of Iroquois and Allied Indians
- Coordinator for the Williams Treaties
- Mississauga of Scugog Island First Nation
- Chippewas of Georgina Island First Nation
- Chippewas of Mnjikaning First Nation
- Hiawatha First Nation
- Beausoleil First Nation
- Alderville First Nation
- Curve Lake First Nation
- Mississaugas of the New Credit First Nations
- Six Nations Haudensaunee Confederacy Council
- Six Nations of the Grand River Territory
- Metis Consultation Unit
- Credit River Métis Council

As the design progressed, First Nations were provided with notification of the PIC and Notice of TESR Addendum Filing.

2.7 Consultation with Utility Companies

The following utility companies were contacted to identify conflicts which may result from the project:

- Bell Canada
- Enbridge Gas Distribution Inc.
- Rogers Cable
- Hydro One
- Trans Northern Pipelines
- Greater Toronto Airport Authority
- Orangeville-Brampton Railway.

As the design progressed, utility companies were provided with notification of the PIC and Notification of Filing of the TESR Addendum.

2.8 Public Information Centre (PIC)

2.8.1 Notification of the PIC

A PIC was held for the Detail Design of the entire Highway 401 expansion project from west of Hurontario Street to the Credit River, including the changes that are the subject of this TESR Addendum.

The Notice of PIC was advertised in English and French in the following newspapers:

- Toronto Star (English) – Wednesday, November 6, 2013
- Mississauga News (English) – Wednesday, November 6, 2013
- L'Express de Toronto (French) – Tuesday, November 5, 2013.

The Notice provided the PIC purpose, location, timing and format; invited stakeholders to attend; and provided the address, telephone number and email address of both the MTO and AECOM Project Managers. The Notice directed individuals to consult the website for project details and material presented at the PIC.

Notification letters (with a copy of the Notice) were mailed on October 30, 2013, to the names on the project contact/ mailing list, including:

- Government agencies and municipalities
- Utility companies
- First Nations.

Affected property owners and individuals that had requested to be added to the mailing list were notified on November 4, 2013.

A copy of the external agency notification materials, including newspaper advertisement, and notification letters are available in **Appendix A**.

2.8.2 PIC Organization and Logistics

The PIC was held on Wednesday, November 13, 2013 at Meadowvale Village Hall, 6970 Second Line West, Mississauga, Ontario between 3:00 pm and 4:00 pm for external agencies and First Nations and between 4:00 pm and 8:00 pm for the general public. The PIC was held to inform attendees of the key findings and recommendations

of the project and to receive and respond to questions, comments and concerns. The overall format for the PIC was an informal drop-in centre setting, whereby participants could view display boards containing information about the project and speak one-on-one with MTO personnel and/or AECOM Project Team representatives. The following display boards were shown at the PIC:

- Welcome to the PIC
- Purpose of the PIC and Purpose of the Project
- Project Background
- Design Features
- Class Environmental Assessment Process
- Existing Environmental Conditions – Natural Environment
- Existing Environmental Conditions – Socio-Economic Environment
- Noise Assessment
- Key Changes from the 2005 TESR
- Fletcher’s Creek Crossing (two (2) slides)
- Lighting System Improvements including Existing and Proposed Illumination Plan (two (2) slides)
- Proposed Construction Staging – Removal of the 2nd Line West;
- Potential Environmental Impacts and Mitigation for the Natural and Socio-Economic and Cultural Environments
- Proposed Belgrave Extension (2 slides)
- Next Steps in the Class EA Process
- How to Remain Involved in the Project.

The presentation materials were uploaded to the project website. In addition, roll plans were presented that displayed the proposed design and construction staging. A copy of the display boards can be seen in **Appendix A**. It is noted that one (1) PIC was held during Detail Design for the entire Highway 401 expansion project from west of Hurontario Street to the Credit River, which includes changes that are the subject of this TESR Addendum.

Participants were encouraged to complete and submit a Comment Sheet to gauge feedback on the project. Comment sheets could be submitted during the PIC or emailed/faxed/mailed to members of the Project Team by December 13, 2013.

Project Team representatives from MTO and AECOM present at the PIC included the following:

Ministry of Transportation	AECOM Canada Limited
Ms. Miao Zhou – Senior Project Engineer Mr. Larry Sarris – Environmental Planner Ms. Astrid Poei – MTO Communications MTO Property Representatives	Mr. Brian Ruck – Project Manager Ms. Mirjana Osojnicky – Environmental Planner Mr. David McNull – Highway Engineer

2.8.3 PIC Attendance

Over the course of the afternoon/evening, twenty-six (26) individuals signed in/attended the PIC. Attendees included local residents, a representative from the Mississauga Fire and Emergency Services, City of Mississauga Councillor and other members of the public interested in the project.

2.8.4 Summary of PIC Participant Feedback

The following agency correspondence was received from the Public Information Centre Notification (**Appendix A**). As noted above, the PIC held in November 2013, was for the entire Highway 401 expansion from west of Hurontario Street to the Credit River. As this TESR Addendum only documents the Belgrave Road connection, the table below does not reflect all comments received at the PIC. Additional comments received at the PIC will be included under separate cover in the Design and Construction Report for the Highway 401 mainline expansion.

Table 1. Agency Correspondence from Public Information Centre Notification

Agency	Comment	Response/Action
Alderville First Nation Letter received November 5, 2013	As per the Alderville First Nation Consultation Protocol, this project is deemed a Level 3, having minimal potential to impact our First Nations' rights. Request to be kept apprised of any archaeological findings, burial sites or any environmental impacts.	Comment noted and will be included in contract documentation.
Peel District School Board Letter received November 8, 2013	Peel District School Board is interested in this project since there is a school in the vicinity of the project area. Request to be informed of project updates.	Will update, as required.
Region of Peel (Transportation) Email received November 11, 2012	Advised of staff changes and request to update contact information.	Project mailing list updated.
Region of Peel (Planning) Email received November 27, 2013	Advised of staff changes and request to update contact information.	Project mailing list updated.
Curve Lake First Nation Email received December 2, 2013	Please notify if human remains or artifacts are encountered.	Comment noted and will be included in contract documentation.
Ministry of Aboriginal Affairs Email received December 11, 2013	Project appears to be located in an area where First Nations may have existing or asserted rights or claims in Ontario's land claims process or litigation that could be impacted by your project. Advised to contact the Mississaugas of the New Credit First Nation.	Mississaugas of the New Credit First Nation are on the project mailing list. No Action.

2.8.4.1 Comments Received

There were no comment sheets submitted during the PIC, however several individuals took comment sheets with them, while others indicated that they may submit comments via email or the project website.

Following the PIC, numerous comments were received from the public; however none of the comments were in relation to the proposed Belgrave Road connection.

2.9 Filing of the TESR Addendum

The Ontario Government Notice of Filing of the TESR Addendum is being placed in the following newspapers:

- Mississauga News (English) – May 29, 2015
- L'Express de Toronto (French) – May 27, 2015

The Notice will announce the commencement of the 30-day public and regulatory agency review period for the TESR Addendum, the locations where the TESR Addendum can be viewed and the closing dates for comments to be submitted.

Notification letters for the filing of the TESR Addendum (with a copy of the Notice) will be provided to the names on the project contact/ mailing list. Copies of the notice and letter are provided in **Appendix A**.

3. Existing Environmental Conditions and Changes

This Addendum must consider the changes that have taken place since the submission of the 2005 TESR. The changes may include new conditions in the project area, new government policies, new engineering standards, or new technologies for mitigation measures.

This section reviews and compares the current environmental conditions with the environmental conditions documented in the 2005 TESR to determine if there are any significant changes that may affect the proposed Belgrave Road connection. Field investigations have been completed to provide an update to current environmental conditions, including, archaeology, terrestrial and species at risk. The collection of updated information was necessary to build upon the existing data and identify the current conditions and constraints to provide a comparison to the 2005 environmental conditions.

New and updated environmental legislation has come into effect since the environmental approval of the 2005 TESR:

- The *Endangered Species Act, 2007* came into force on June 30th, 2008. Species at Risk and their habitats in the study area are thus, subject to stringent regulation. There are no Species at Risk identified within the Belgrave Road connection (Mavis Road/Highway 401 interchange) study area.
- Technical requirements for Noise have been updated in the *MTO Environmental Guide for Noise, October 2006*. A noise assessment was undertaken during the preliminary design phase (2005) and updated during Detail Design (2014). Based on the noise investigations, it has been determined that there is no warrant for noise mitigation under the MTO's Highway 401 widening project.
- In 2011, the Ministry of Tourism Culture and Sport released updated *Standards and Guidelines for Consultant Archaeologists* for the completion of archaeological assessments. As further discussed in Section 5.3.1, a Stage 2 Archaeological Assessment was completed in 2012 which included the Mavis Road interchange area. Despite careful scrutiny, no archaeological remains were encountered. The project area was cleared of further archaeological concern.

4. Proposed TESR Design Changes

4.1 Belgrave Road Connection Alternatives

As noted in Section 1, the Ministry of Transportation (MTO) investigated the need for a future connection of Belgrave Road from the existing cul-de-sac west of Suffolk Court westerly to the Highway 401 eastbound off ramp at Mavis Road – a distance of approximately 200 m.

Three (3) alternatives for a future extension of Belgrave Road from the Highway 401 eastbound off ramp at Mavis Road easterly to the existing cul-de-sac west of Suffolk Court were developed for a comparative evaluation.

4.1.1 Alternative A – Mavis Road Underpass to Belgrave Road

The full grade separated option would provide unimpeded unidirectional traffic flow onto Belgrave Road from the west north/south ramp via a tunnel under the existing Mavis Road. Access from Highway 401 west would be split into two ramps when approaching Mavis Road. The west north-south ramp would approach Mavis Road at-grade while a second west-east ramp would tunnel under Mavis Road [and southeast ramp] to connect to Belgrave Road (Figure 3).

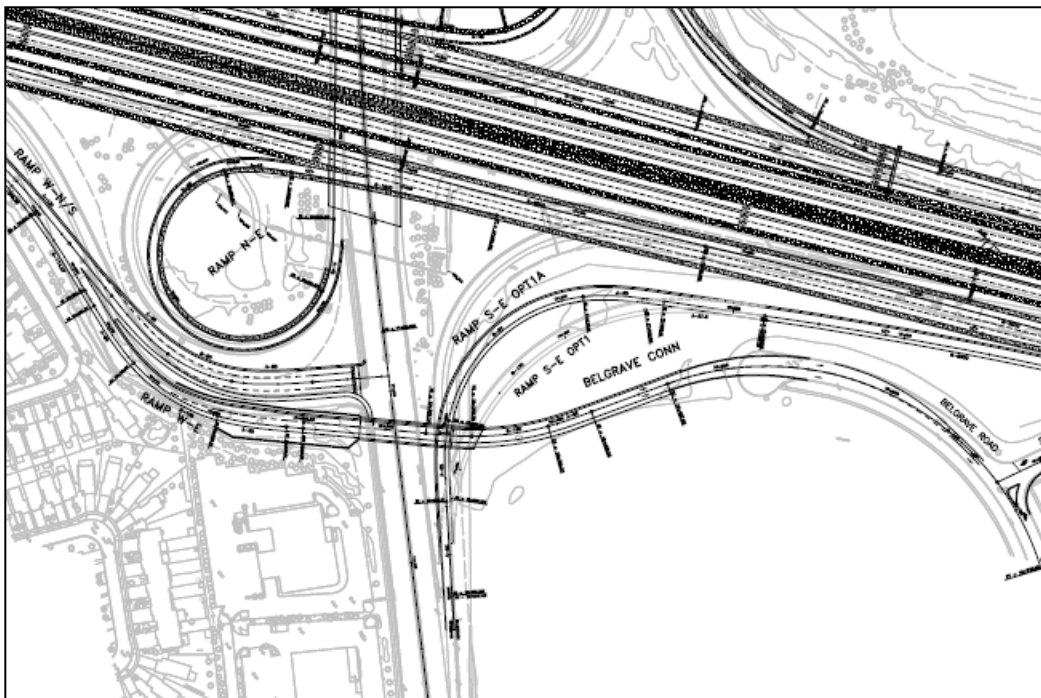


Figure 3. Belgrave Road Alternative A – Plan View

Evaluation

Although this alternative is achievable from a geometric perspective, there are several constraints and issues associated with this concept which are generally attributed to the challenges of deep cuts in developed areas.

Advantages:

- No changes to traffic operations
- Ability to accommodate large volumes of traffic, thus improve traffic flow.

Disadvantages:

- Very costly due to:
 - Construction of retaining walls
 - A large tunnel structure
 - Drainage challenges
 - Illumination requirements
 - Utility conflicts and relocations
- Future construction staging issues with Mavis Road traffic and interchange improvements
- Provides only 1-way eastbound access to Belgrave Road.
- Not easily incorporated into the Mavis Road interchange in the future.

4.1.2 Alternative B – At Grade Intersection with Mavis Road Structure over Realigned Ramp S-E

Alternative B would provide an at-grade connection allowing 2-way traffic on Belgrave Road from Mavis Road forming a 4-leg intersection at the existing intersection of the Highway 401 eastbound off-ramp/Mavis Road. The westbound through movement at the Belgrave Road/Mavis Road intersection is restricted. Some turning movements would also be restricted at the intersection (i.e., Belgrave Road westbound Left) to optimize traffic operations.

A south to east at-grade ramp would be constructed under the Belgrave Road extension. MTO would construct the south-east ramp on a lower profile and the City would build a bridge over the ramp to connect Belgrave Road at a later date (Figure 4).

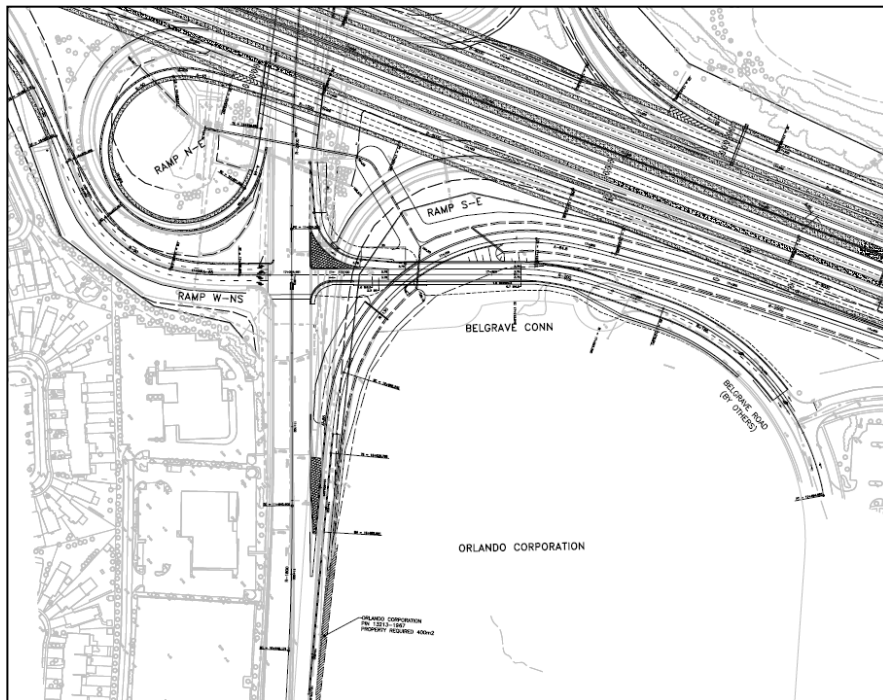


Figure 4. Belgrave Road Alternative B – Plan View

Evaluation

Advantages:

- Less costly than Alternative A due to requirements of a small bridge structure and limited utility considerations
- Allows 2-way traffic on Belgrave Road
- Permits access to Mavis Road from Belgrave Road (westbound Belgrave Road to northbound Mavis Road only)
- Easily accommodated in future design
- Limited traffic disruption during construction.

Disadvantages:

- Minor geometric/operational modifications to terminal intersection.

4.1.3 Alternative C – At-Grade Intersection with Mavis Road Infrastructure over Realigned Ramp S-E

Alternative C would provide a similar road alignment to Alternative B at the intersection of the Highway 401 eastbound off-ramp/Mavis Road with the relocation of the existing Highway 401 eastbound on-ramp to the north of the Highway 401 eastbound off-ramp/Mavis Road intersection. MTO Central Region has utilized this type of connection within Parclo-A type interchanges in some recent highway projects to maintain connections to existing connected roadways.

Construction of a tighter southeast Ramp avoids the construction of a bridge structure (Figure 5).

The westbound through movement at the Belgrave Road /Mavis Road intersection is restricted. Some turning movements would also be restricted at the intersection (i.e. Belgrave westbound Left) to optimize traffic operations.

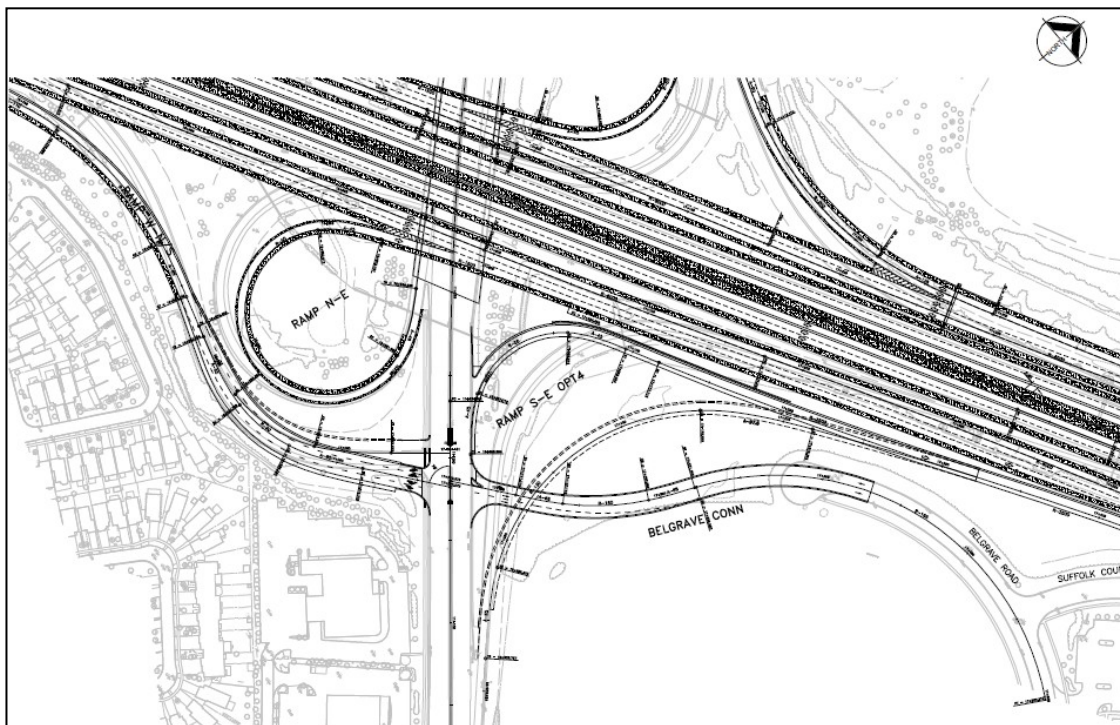


Figure 5. Belgrave Road Alternative C – Plan View

Evaluation*Advantages:*

- Least cost due to no bridge structures, some retaining wall, limited utility impacts
- Allows for 2-way traffic on Belgrave Road
- Permits access to Mavis Road from Belgrave Road (westbound Belgrave Road to northbound Mavis Road only).

Disadvantages:

- Significant geometric compromises for southeast Ramp
- Terminal intersection skew
- A full intersection is not preferred because this is an area with heavy traffic volume
- Additional property may be required.

4.1.4 Recommended Alternative

A summary comparison of the key advantages/disadvantages of Alternatives A, B and C is provided in the Table 2.

Table 2. Summary of Key Advantages/Disadvantages of Alternatives A, B and C

Advantages / Disadvantages	Alternative A: Mavis Road Underpass to Belgrave Road	Alternative B: At-Grade Intersection with Mavis Road Structure Over Realigned S-E Ramp	Alternative C: At-Grade Intersection with Mavis Road Infrastructure Over Realigned S-E Ramp
Directional Traffic Access at Belgrave Road	<ul style="list-style-type: none"> • Provides only 1-way eastbound traffic access to Belgrave Road 	<ul style="list-style-type: none"> • Allows 2-way traffic on Belgrave Road, permitting access from westbound Belgrave Road to northbound Mavis Road only 	<ul style="list-style-type: none"> • Allows 2-way traffic on Belgrave Road, permitting access from westbound Belgrave Road to northbound Mavis Road only
Ease of Incorporation into the Mavis Road Interchange in the Future	<ul style="list-style-type: none"> • Difficult to incorporate into the Mavis Road interchange in the future. 	<ul style="list-style-type: none"> • Easily accommodated into the Mavis Road interchange in the future 	<ul style="list-style-type: none"> • Easily accommodated into the Mavis Road interchange in the future
Other Design Considerations	<ul style="list-style-type: none"> • Ability to accommodate large volume of traffic. Drainage challenges. • Utility conflicts and relocation required • Large retaining walls required 	<ul style="list-style-type: none"> • None 	<ul style="list-style-type: none"> • Tighter southeast ramp to avoid the need for a bridge structure imposes geometric compromises
Traffic Disruption During Construction	<ul style="list-style-type: none"> • Significant traffic disruption during construction associated with underpass construction 	<ul style="list-style-type: none"> • Limited traffic disruption during construction 	<ul style="list-style-type: none"> • Limited traffic disruption during construction
Construction Cost	<ul style="list-style-type: none"> • Highest construction cost 	<ul style="list-style-type: none"> • Lower construction cost than Alternative A 	<ul style="list-style-type: none"> • Lowest construction cost due to no bridges
Change in Footprint Impact (key change in environmental impact from that presented in the 2005 TESR)	<ul style="list-style-type: none"> • Requires most significant realignment of the eastbound off ramp in a southerly direction, placing it closer proximity to adjacent development • Extension of Belgrave Road has larger footprint impact than Alternative B to lands in SE quadrant 	<ul style="list-style-type: none"> • Requires minimal (least) realignment of the eastbound off ramp in a southerly direction, placing it closer proximity to adjacent development • Extension of Belgrave Road has smallest footprint impact to lands in SE quadrant 	<ul style="list-style-type: none"> • Requires some realignment of the eastbound off ramp in a southerly direction, placing it closer proximity to adjacent development • Extension of Belgrave Road has larger footprint impact than Alternative B to lands in SE quadrant • Tighter southeast ramp to avoid the need for a bridge structure has smaller footprint impact to lands in SE quadrant
	Not Recommended	Recommended	Not Recommended

Alternative B is selected to be carried forward as the Recommended Plan. This alternative has been selected due to:

- Operational benefits including providing free flow of traffic and provision of a 'right out' access to northbound Mavis Road
- Allows for the protection of a future Belgrave extension within the current MTO contract.

The key changes to the design of Highway 401 interchange at Mavis Road documented in the 2005 TESR are the following:

- An at-grade connection allowing 2-way traffic on Belgrave Road from Mavis Road will be provided by forming a 4-leg intersection at the existing intersection of the Highway 401 eastbound off-ramp at Mavis Road. Some turning movements will be restricted at the intersection (i.e., Belgrave Road westbound Left) to optimize traffic operations.
- The south to east at-grade ramp will be constructed at a lower profile under the Belgrave Road extension and a bridge will be constructed over the ramp to connect Belgrave Road.
- The Belgrave Road extension will be comprised of two (2) eastbound lanes and one (1) westbound lane each at a 3.75m width, with one of the eastbound lanes becoming a left turn lane at the next intersection. A sidewalk will be provided to allow pedestrian access from Mavis Road to Belgrave Road.
- MTO will construct the ramp and the City of Mississauga will construct the bridge and the extension to Belgrave Road at a later date.

Appendix B provides a typical cross-section, plan and profile drawings of the recommended plan, as well as the General Arrangement drawing for the Belgrave Road structure over the southeast ramp.

5. Environmental Concerns and Proposed Mitigation

This section describes any environmental effects from the modifications to the original EA design of the Belgrave Road connection. This section also includes proposed mitigation to specific impacts.

5.1 Natural Environment

5.1.1 Fisheries and Aquatic Habitat

There are no watercourses in the vicinity of the Belgrave Road extension.

5.1.2 Wildlife

Terrestrial surveys were completed on June 10th, and November 11th, 2011, as well as, on February 15th and 16th and May 24th and 25th, 2012. Wildlife was not observed in the vicinity of the Belgrave Road connection (Mavis Road/Highway 401 interchange).

In the event that wildlife are present in the study area, vegetation and site clearing will take place between September 1st and March 31st and avoid the months of April until August, in accordance with the *Migratory Birds Convention Act*.

5.1.3 Vegetation

An Ecological Land Classification (ELC) Assessment was completed for the entire project area in the 2005 TESR and updated in 2011 and 2012.

Land in the southeast corner of the Highway 401 and Mavis Road interchange is dominated by herbaceous vascular plant species with a mixture of grasses and forbs. The areas in which these plants occur are constrained within a thin band of land adjacent to development. These communities are highly influenced by development and contain a high percentage of invasive species.

CUM 1-1: Cultural Meadow – This community dominates the area and is comprised of species typical of cultural meadow areas. Species include a mixture of Kentucky bluegrass (*Poa pratensis*), wild carrot (*Daucus carota*), common dandelion (*Taraxacum officinale*), Canada thistle (*Cirsium arvense*), clover (*Trifolium repens*), goldenrod species (*Solidago sp*), crown vetch (*Coronilla varia*), common milkweed (*Asclepias syriaca*), oxeye daisy (*Chrysanthemum leucanthemum*), teasel (*Dipsacus sylvestris*), bull thistle (*Cirsium vulgare*), quack grass (*Agropyron repens*), bittercress (*Cardamine hirsute*), giant foxtail grass (*Setaria faberii*), orchard grass (*Dactylis glomerata*), curled dock (*Rumex crispus*) and shepherd's purse (*Capsella bursa-pastoris*). Dominant plant forms (i.e. grasses or forbs) alternate in areas, but overall the area is a mix of the two. All species are either, common, non-native or invasive.

MAM2-2: *Reed Canary Grass Mineral Meadow Marsh Type* – This community occurs within the ditch and contains reed canary grass (*Phalaris arundica*), common reed grass (*Phragmites australis*) and cattail (*Typha latifolia*). Closer to Mavis, a patch of purple loosestrife (*Lythrum salicaria*) was noted. No standing water was observed in the meadow marsh areas. The ditch area temporarily holds water from rain events to sustain these very common plant species. No permanent standing water was observed.

5.1.4 Wetlands

There are no wetlands in the vicinity of the Belgrave Road extension.

5.1.5 Drainage

The most significant drainage impact as a result of the design is attributed to the deep cut of the southeast ramp under the Belgrave Road connection. This will be addressed as part of the southeast ramp design completed by MTO. Typical urban drainage design will be implemented for the Belgrave Road connection design.

5.1.6 Erosion and Sediment Control

The potential for significant erosion is limited due to the absence of watercourses within the project area. Nevertheless, erosion and sediment control practices will be implemented during construction to minimize the risk of erosion and sediment loading.

As noted during the 2005 TESR, erosion and sediment control practices will focus on two separate targets: minimizing site erosion and keeping any eroded material on site. All permanent and temporary erosion and sediment control measures will be identified in the contract drawings where applicable, and will be included within the Contract specifications, during detail design. Such measures will include, among others:

- Limiting vegetation removal to only what is required for grading and ditching operations
- Constructing embankment slopes at a maximum ratio of 2 horizontal to 1 vertical
- Establishing cover as soon as possible after excavation.

5.2 Socio-Economic Environment

5.2.1 Noise

Traffic noise predictions indicate that the project is expected to have a low to medium perceived noise impact at the Mavis Road and Highway 401 interchange.

Construction noise varies based upon a variety of factors such as time and location of operation, size and concurrent use of equipment and staging of construction.

Noise levels are enforced within the City of Mississauga through Noise Control By-law No. 360-79, as amended. A requirement for public notification will be included in the contract documentation for affected residents within a 500 m radius prior to any overnight construction activities. Any complaints about noise will be investigated in accordance with the MTO/MOECC Noise Protocol.

Standard contract provisions regarding restrictions on operations causing significant construction noise will be confirmed during Detail Design.

5.2.2 Property Waste and Contamination

A Phase I Environmental Site Assessment was conducted in 2014 for the MTO, on behalf of the City of Mississauga for due diligence purposes.

The following were identified as Potentially Contaminating Activities (PCAs) that have the potential to impact the soil and groundwater beneath the property:

- Historical spills and roadway maintenance (historical and current) on adjacent Highway 401 and adjacent Highway 401 eastbound on-ramp.
- Historical and current roadway maintenance on adjacent Mavis and Belgrave Road.

Based on the PCAs discussed above and the size of the property, the entire property was identified as an Area of Potential Environmental Concern (APEC). A Phase II ESA is recommended to investigate the potential soil and groundwater impacts prior to construction.

5.2.3 Air Quality

During construction dust and emissions will be generated by construction equipment and activities. Standard mitigation and best management practices, such as a dust control plan, will be applied during construction.

5.2.4 Property

The project area property has been acquired by the MTO or under the ownership of the City of Mississauga; therefore no private property impacts are anticipated.

5.2.5 Utility

The east side of Mavis Road contains the following utilities:

- Rogers Cable
 - Utility records indicate that Rogers owns a buried fiber optic cable along the east side of Mavis Road. This will require advanced relocation to facilitate the roadway construction and associated works.
- Bell Canada
 - Utility records indicate that Bell owns buried plant along the east side of Mavis Road. This will require advanced relocation to facilitate the roadway construction and associated works.
- Enersource Hydro
 - The Enersource Hydro conflicts are primarily limited to service connections to existing illumination for Mavis Road and the end of the existing Belgrave Road cul-de-sac. These will need to be addressed in the electrical/illumination design as well as the traffic signal design. Several existing light standards will require relocation as a result of this work.
 - One existing span guy pole and anchors is also in conflict with the proposed extension.

5.3 Cultural Environment

5.3.1 Archaeological Resources

Building upon recommendations made in the 2005 TESR, a Stage 2 Archaeological Assessment (AA), from Highway 403/410 Interchange to the Credit River was completed in August 2012. This assessment included the Mavis Road/Highway 401 interchange; however it did not include the parcel of land required for the Belgrave extension, since it was outside of the scope of MTO's highway widening project. It is recognized that the land required for the Belgrave Road extension contains numerous disturbances; however a Stage 1/2 archaeological assessment will need to be completed by the City of Mississauga during Detail design so that archaeological clearance can be obtained prior to tendering.

5.3.2 Built and Cultural Heritage Resources

No mitigation measures are recommended for Highway 401 corridor in the vicinity of the Belgrave Road extension area as previous widening activities have altered the original design and thus, resulted in the paving of the grass median (Unterman McPhail Associates, 2013).

5.4 Traffic

The introduction of the new connection between the existing cul-de-sac west of Suffolk Road and the Highway 401 eastbound off ramp at Mavis Road will benefit traffic operations at the intersection of Mavis Road/Bancroft Drive/Cantay Road by providing an alternative route for commuters travelling to/from the east of Mavis Road, as

documented in the Mavis Road Traffic Diversion Analysis (AECOM, 2014). The capacity analysis results indicate that both the overall Level of Service and individual movements, particularly with respect to westbound right-turn and southbound left-turn, at intersection of Mavis Road/Bancroft Drive/Cantay Road, will be significantly improved under existing and future conditions.

5.5 Summary of Environmental Protection / Mitigation Requirements Prior to and During Construction

The following table provides a summary of the key environmental concerns and mitigation for the Belgrave Road extension.

Table 3. Summary of Environmental Protection / Mitigation Requirements Prior to and During Construction

I.D.#	Issues/Concerns Potential Effects	Concerned Agencies	I.D.#	Mitigation/Protection/Monitoring
General Environmental Protection Requirements				
1	General Environmental Protection	MTO	1.1	<ul style="list-style-type: none"> The general requirement to protect the natural environment from impacts and damage, including physical damage, emissions, and local access; and Control of materials, equipment, construction method, construction disturbance limits, and work timing/staging duration, in order to achieve the foregoing.
Natural Environment Concerns and Commitments				
2	Fisheries and Watercourse Impacts	MNRF	2.1	<ul style="list-style-type: none"> No watercourse within study area; and No mitigation required.
3	Wildlife Impacts (low probability)	MNRF	3.1	<ul style="list-style-type: none"> Temporary vegetation protection fencing installed at the edge of the clearing / grubbing limits; Requirement that construction equipment and vehicles give a right-of-way to wildlife, allowing wildlife to pass and proceed to a safe distance prior to construction equipment/vehicles commencing construction activities; and Requirement that work cease and Contract Administrator be notified in the event wildlife is injured during construction activities.
4	Bird Impacts (in long grass)	MNRF	4.1	<ul style="list-style-type: none"> Vegetation and site clearing will take place between September and March 31st and avoid the months of April until August, in accordance with the <i>Migratory Birds Convention Act</i>.
5	Potential to encounter Species at Risk during construction	MTO/MFNR	5.1	<ul style="list-style-type: none"> If Species at Risk is encountered a qualified biologist shall be contacted in order to determine how to proceed without contravening the ESA (2007).
6	Vegetation impacts	MNRF	6.1	<ul style="list-style-type: none"> Vegetation removal will be kept to a minimum and exposed soils will be re-vegetated with an approved MTO seed mix that suits the area soils; and immediately following the completion of required work and the final grade has been established.
7	Erosion and sediment control	MTO	7.1	<ul style="list-style-type: none"> An erosion and sediment control plan for construction shall be developed and effectively implemented. It will include: <ul style="list-style-type: none"> Limiting vegetation removal to only what is required for grading and ditching operations; Constructing embankment slopes at a maximum ratio of 2:1; and Establishing cover as soon as possible after excavation. Sediment fences around construction activities.

I.D.#	Issues/Concerns Potential Effects	Concerned Agencies	I.D.#	Mitigation/Protection/Monitoring
Socio-Economic Environment Concerns and Commitment				
8	Construction Noise	MTO/MOECC	8.1	<ul style="list-style-type: none"> Idling of construction equipment will be kept to a minimum and maintained in good working order; and Contract documents provided to the contractor will contain general noise control measures to mitigate the noise impact at noise sensitive areas including two standard clauses regarding equipment noise.
			8.2	<ul style="list-style-type: none"> Respond to construction noise complaints according to MTO/MOECC "Noise Protocol".
9	Dust Control during Construction	MTO/MOECC	9.1	<ul style="list-style-type: none"> A dust control plan for construction shall be developed and effectively implemented.
10	Spill Prevention and Response	MTO	10.1	<ul style="list-style-type: none"> Contractor shall have a spill prevention and response contingency plan that addresses procedures for preventing and responding to spills, and the equipment (including a spill response kit) and resources that will be available.
			10.2	<ul style="list-style-type: none"> Refuelling precautions will be taken; and Refuelling will be conducted only in designated areas preferably situated on an impermeable surface.
			10.3	<ul style="list-style-type: none"> Construction equipment required to be in good condition.
11	Management of Surplus Material and Debris	MTO	11.1	<ul style="list-style-type: none"> Management materials will be dealt with in accordance with normal MTO practices (regulated by OPSS 180).
			11.2	<ul style="list-style-type: none"> A Phase II ESA will be completed during Detail Design prior to tendering.
12	Potential for temporary or permanent change to emergency access	OPP/Peel Region	12.1	<ul style="list-style-type: none"> MTO will notify OPP and emergency medical services of construction staging, start of construction etc. to minimize delay in emergency response times during and after construction.
13	Potential impacts to existing utilities	Rogers Cable Bell Canada Enersource Hydro	13.1	<ul style="list-style-type: none"> Utility relocation will be coordinated prior to construction.
Cultural Environment Concerns and Commitments				
14	Impacts to archaeological resources	MTCS	14.1	<ul style="list-style-type: none"> A Stage 2 Archaeological Assessment will be completed by the City of Mississauga during detail design so that archaeological clearance can be obtained for the extension prior to tendering.
			14.2	<ul style="list-style-type: none"> During construction, if any archaeological and/or historical resources are discovered, they may be a new archaeological site and therefore subject to Section 48(1) of the <i>Ontario Heritage Act</i>. Work in the area would not resume until cleared by the Ministry of Tourism, Culture and Sport.
15	Impact to cultural heritage landscape (Highway 401 Roadscape)	MTCS	15.1	<ul style="list-style-type: none"> No mitigation required.

6. Environmental Monitoring / Inspection Program for Project Implementation

6.1 Overview of Environmental Monitoring / Inspection Program for Project Implementation

Environmental monitoring / inspection is a component of the overall monitoring / inspection program for project implementation.

The environmental monitoring / inspection for project implementation has the following components:

- On site construction administrator to oversee the work
- Environmental monitoring plans
- Design package handover meeting
- Pre-construction notifications
- pre-construction environmental monitoring / inspection
- environmental monitoring during construction
- post-construction environmental monitoring / inspection.

As part of the MTO's continuing commitment to environmental stewardship, comprehensive construction and post-construction monitoring programs will be instituted on this undertaking. The programs will assess the effectiveness of environmental mitigation and compensation measures, as well as the degree of compliance with environmental mitigation and protection measures committed to during the construction period.

6.2 Environmental Monitoring / Inspection During Construction

During construction, the Contractor and Contract Administrator will ensure that implementation of mitigating measures and key design features are consistent with contractual requirements and commitments to external stakeholders. This will be achieved through the establishment of an environmental inspection approach that includes the use of an Environmental Inspector operating under the direction of the Contract Administrator, in compliance with MTO's Construction Administration and Inspection Task Manual (May 2014) (CAIT Manual). The CAIT Manual task descriptions provide guidance to those involved in inspection on MTO construction contracts, to ensure that the quality and quantity of the work is in accordance with Ministry specifications, standards, drawings, policies and procedures.

The project will be subject to periodic on-site environmental inspection to ensure that the execution of the environmental component of the work is occurring as planned and to deal with any environmental problems that may develop during construction. The periodic on-site inspection will be supplemented by the support of environmental specialists retained by the Contract Administrator, as required, to assure the proper implementation of site-specific mitigation or remediation measures.

In addition, the effectiveness of the environmental protection measures will be assessed to ensure that:

- Environmental protection measures required in the pre-construction (site preparation) phase are implemented and monitored during the construction activities (e.g., inspection for nesting migratory birds)
- Individual mitigation measures are providing the expected control and/or protection (e.g., dust and noise control);
- Composite control and/or protection provided by the mitigating measures is adequate (e.g., Erosion and Sediment Control Plan; winter shutdown)
- Deficiencies (poor installation; ineffectiveness) are corrected in an effective and timely manner

- Additional/enhanced mitigation measures are provided, as required, for any unanticipated environmental problems that may develop during construction, and appropriate contingency and emergency response plans are in place and being followed (e.g., spills control, discovery of archaeological artifacts).

To assist the Environmental Inspector and standardize the environmental inspection duties, a verification/checklist will be developed, listing all mitigation measures and commitments to be undertaken to ensure compliance with such commitments associated and requirements. Any concerns and required action will be detailed in a standard inspection report form and signed off on as the concerns are addressed or the additional work is completed.

The Environmental Inspector will have the mandate to verify that all environmental protection and mitigation/compensation measures are implemented. Through the Contractor's Site Supervisor, the Environmental Inspector will have the authority to direct the construction crews to interrupt specific project activities if the proper mitigation measures are not in place. In the event that a mitigation measure could be modified to better protect the environment (public and workers included), the inspector will have the authority to notify the Contract Administrator. Any such proposed interruptions or notifications will be recorded by the Environmental Inspector and promptly reported to the Contract Administrator and MTO Project Manager. The inspector will give immediate notification to the Project Manager if, in his/her opinion, there is failure to take appropriate action by the Contract Administrator.

The Environmental Inspector will also monitor areas outside of the highway corridor (or site) that have been identified as potentially sensitive to either direct or indirect impacts of the highway construction (e.g., private properties and downstream reaches of watercourses).

6.3 Post-Construction Environmental Monitoring / Inspection

The purpose of the post-construction monitoring and environmental inspection program is to ensure, to the extent possible, that lands disturbed as a result of construction activities will be restored to their original use and condition as soon as possible after construction. At this time, it is anticipated that the following restoration activities will be carried out:

- All disturbed areas have been graded as required
- All disturbed areas have been seeded
- If after 60 days the seeded areas are not stabilized ensure that the Contractor re-applies seed and mulch.

In addition, to site-specific monitoring requirements, MTO will undertake an audit of the environmental performance of this project. The audit will include, but not be limited to:

- Review of the long-term effectiveness of mitigation measures
- Review of the inspection reports, notes and the resolution of noted concerns
- Review of comments and concerns received from regulatory agencies and public interest groups and how these issues were addressed
- Recommended modifications to mitigation measures or procedures as required.

Appendix A

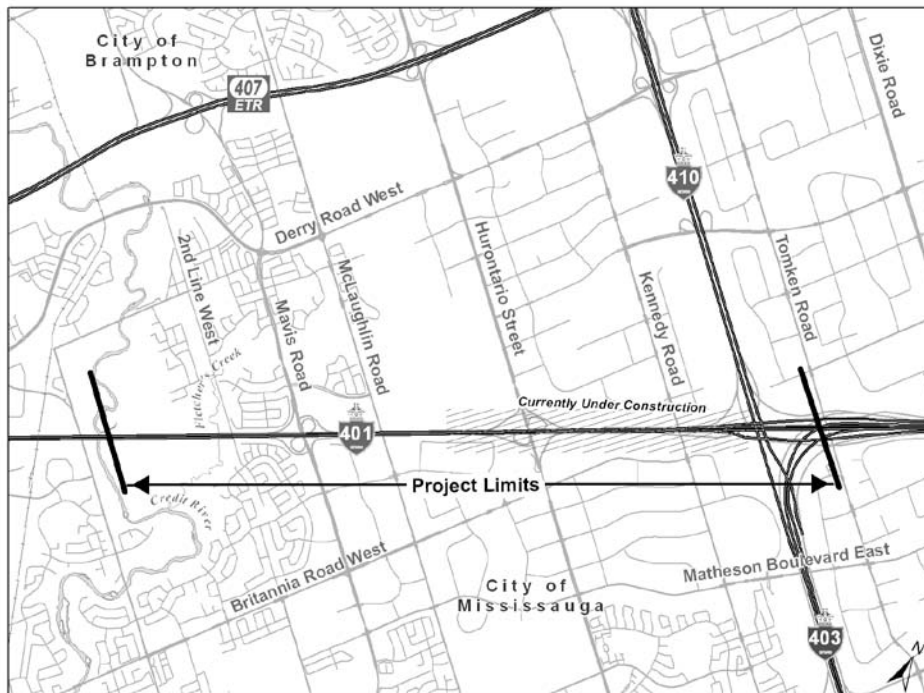
Consultation



Notice of Commencement

**HIGHWAY 401 WIDENING
FROM HIGHWAY 403/410 INTERCHANGE TO THE CREDIT RIVER
DETAIL DESIGN G.W.P. 2150-01-00**
THE PROJECT

AECOM Canada Ltd. (AECOM) has been retained by the Ontario Ministry of Transportation (MTO) to undertake the Detail Design Project for the ultimate widening of Highway 401 from the Highway 403/410 interchange to the Credit River in the City of Mississauga, Regional Municipality of Peel, a distance of 7 km (see Key Map). The project will complete the expansion of Highway 401 from Highway 410/403 interchange to the Credit River, from its current 6 lanes to a 12-lane core/collector system including a High Occupancy Vehicle (HOV) lane in each direction.

Key Map

THE PROCESS

The project will follow the approved environmental planning process for Group 'B' projects under the *Class Environmental Assessment for Provincial Transportation Facilities* (2000).

A Transportation and Environmental Study Report (TESR) was prepared in August 2005 and received environmental approval in July 2007. A review of this TESR will be conducted for this project. A Design and Construction Report (DCR) will be prepared to document environmental investigation, environmental impacts, proposed mitigation measures and commitments to future work. This document will be made available for public review upon completion of Detail Design.

Project information will be posted and updated on the project website: www.401expansion-mississauga.ca

A Public Information Centre (PIC) will be held for this project to present details of the proposed Highway 401 widening, construction staging, and mitigation measures to address potential environmental impacts. Notice of the PIC will be provided in local newspapers and on the project website. The PIC is anticipated to be held in the summer of 2012.

COMMENTS

There is opportunity at any time of the project for interested persons to provide comments and input to the Project Team. Comments and information regarding this project are being collected to assist in meeting the requirements of the *Environmental Assessment Act*, in accordance with the *Freedom of Information Act*. This material will be maintained on file for use during the project and may be included in project documentation. With the exception of personal information, all comments will become part of the public record.

To obtain additional information, provide comments, or to join our mailing list, please contact either one of the following Project Team members:

Miao Zhou, M.Eng. P.Eng.
Senior Project Engineer
Ontario Ministry of Transportation
Central Region
Building 'D', 4th Floor
1201 Wilson Avenue
Downsview, ON M3M 1J8
Phone: (416) 235-4277
Fax: (416) 235-3576
Email: Miao.Zhou@ontario.ca

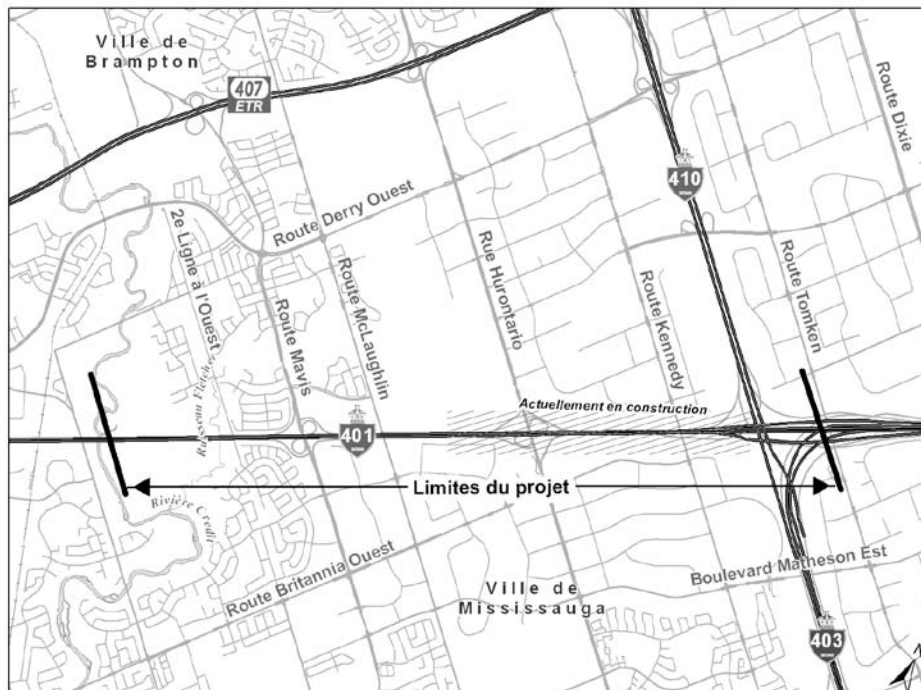
Brian Ruck, P. Eng. C.V.S.,
Consultant Project Manager
AECOM Canada Ltd.
300 Water Street
Whitby, ON L1N 9J2
Toll Free: 1-800-668-1983 or
Phone: (905) 668-4021 ext. 2250
Fax: (905) 665-4867
E-mail: Brian.Ruck@aecom.com

ÉLARGISSEMENT DE L'AUTOROUTE 401
ENTRE L'ÉCHANGEUR DE L'AUTOROUTE 403/410 ET CREDIT RIVER
CONCEPTION DÉTAILLÉE G.W.P. 2150-01-00

LE PROJET

Le ministère des Transports de l'Ontario (MTO) a retenu les services d'AECOM Canada Ltd. pour entreprendre le projet de conception détaillée pour l'élargissement ultérieur de l'autoroute 401 entre l'échangeur de l'autoroute 403/410 et la rivière Credit River dans la ville de Mississauga, Municipalité régionale de Peel, sur une distance de 7 km (voir la carte-index). Le projet complètera l'agrandissement de l'autoroute 401 entre l'échangeur de l'autoroute 403/410 et la rivière Credit River, de 6 voies actuellement en une autoroute principale/collectrice de 12 voies comprenant une voie réservée aux véhicules multioccupants (VMO) dans chaque direction.

Carte index



PROCESSUS

Cette étude se déroulera conformément au processus approuvé de planification environnementale pour les projets du groupe B décrits dans l'*Évaluation environnementale de portée générale pour les installations provinciales de transport (2000)*.

Un Rapport d'étude environnementale pour le transport (REET) a été rédigé en août 2005 et a obtenu l'approbation environnementale en juillet 2007. Ce REET sera réexaminé dans le cadre de ce projet. Un Rapport de conception et de construction (RCC) sera rédigé pour documenter l'étude sur l'environnement, les répercussions sur l'environnement, les mesures d'atténuation proposées ainsi que les engagements pour des travaux futurs. Une fois la conception détaillée achevée ce document sera rendu public pour un examen.

Les informations concernant le projet seront affichées et mises à jour sur le site Web du projet : www.401expansion-mississauga.ca

Un Centre d'information (CI) sera ouvert au public pour ce projet afin de présenter les détails concernant l'élargissement de l'autoroute 401, la planification de la construction, et les mesures d'atténuation pour traiter des répercussions potentielles sur l'environnement. Un avis concernant le CI sera publié dans les journaux locaux et sur le site Web du projet. Il est prévu que le CI sera tenu pendant l'été en 2012.

COMMENTAIRES

Les personnes intéressées pourront à n'importe quel moment pendant le projet, faire des commentaires ou apporter leur contribution à l'équipe du projet. Les commentaires et les informations concernant ce projet sont recueillis afin d'aider à satisfaire les exigences de la *Loi sur les évaluations environnementales*, en conformité avec la *Loi sur l'accès à l'information*. Ces éléments seront conservés dans le dossier pour être utilisés pendant le projet et pourront être inclus dans la documentation du projet. À l'exception des informations personnelles, tous les commentaires feront partie des dossiers publics.

Pour obtenir de plus amples renseignements, faire des commentaires, ou vous inscrire sur notre liste d'abonnement, veuillez communiquer avec l'un des membres suivants de l'équipe du projet :

Miao Zhou, M.Eng. P.Eng.

Ingénieur principal chargé de projet
Ministère des Transports de l'Ontario
Région du Centre
Immeuble D, 4ème étage
1201 Wilson Avenue
Downsview, ON M3M 1J8
Téléphone : 416 235-4277
Télécopieur : 416 235-3576
Courriel : Miao.Zhou@ontario.ca

Brian Ruck, P. Eng. C.V.S.,

Chargé de projet de l'expert-conseil
AECOM Canada Ltd.
300 Water Street
Whitby, ON L1N 9J2
Numéro sans frais : 1 800 668-1983 ou
Téléphone : 905 668-4021, poste 2250
Télécopieur : 905 665-4867
Courriel : Brian.Ruck@aecom.com

Affiliation	Agency	Sub-Agency	Title	Contact Names	Greeting	Street Address	City	Prov	P.C.	Phone No.	Fax No.	Email	NoC Electronic Notification Date	NoC Hard Copy Notification Date
Federal	Canadian Environmental Assessment Agency	Ontario Region	CEAA Coordinator	Mr. Jim Chan	Mr. Chan	55 St. Clair Ave. E., 9th Flr	Toronto	ON	M4T 1M2			jim.chan@ceaa-acee.gc.ca	6/21/2011	6/28/2011
Federal	Canadian Environmental Assessment Agency	Ontario Region	Regional Director	Ms. Louise Knox	Ms. Knox	55 St. Clair Ave. E., 9th Flr	Toronto	ON	M4T 1M2			louise.knox@ceaa-acee.gc.ca	6/21/2011	6/28/2011
Federal	Fisheries and Oceans Canada	Burlington Office	Referral Coordinator		Sir/Madam	304-3027 Harvester Rd, PO Box 85060	Burlington	ON	L7R 4K3			info@dfo-mpo.gc.ca	6/21/2011	6/28/2011
Federal	Fisheries and Oceans Canada	Fish Habitat Management, Ontario-Great Lakes Area	Senior Habitat Biologist	Ms. Sara Eddy	Ms. Eddy	867 Lakeshore Rd.	Burlington	ON	L7R 4A6			sara.eddy@dfo-mpo.gc.ca	6/21/2011	6/28/2011
Federal	Fisheries and Oceans Canada		Inspection Officer			201 N. Front St, Suite 203	Sarnia	ON	N7T 8B1				6/21/2011	6/28/2011
Federal	Environment Canada	Environmental Protection Operations Division - Ontario Region	Manager, Environmental Assessment Section	Mr. Rob Dobos	Mr. Dobos	867 Lakeshore Rd, P.O. Box 5050	Burlington	ON	L7R 4A6			rob.dobos@ec.gc.ca	6/21/2011	6/28/2011
Federal	Aboriginal Affairs and Northern Development Canada	Environmental Assessment Coordination	Environmental Unit			25 St. Clair Ave E, 8th Flr	Toronto	ON	M4T 1M2			Eacoordination_ON@inac-ainc.gc.ca	6/21/2011	6/28/2011
Federal	Aboriginal Affairs and Northern Development Canada	Litigation Management and Resolution Branch	Team Leader, Ontario/Nunavut Litigation Team	Ms. Josee Beauregard	Ms. Beauregard	10 Wellington St. Rm 1310	Gatineau	QC	K1A 0H4			josee.beauregard@ainc-inac.gc.ca	6/21/2011	6/28/2011
Federal	Aboriginal Affairs and Northern Development Canada	Office of the Federal Interlocutor for Metis and Non-status Indians	Senior Policy Analyst	Mr. Jeffrey Betker	Mr. Betker	66 Slater St., Rm 1218	Ottawa	ON	K1A 0H4			jeffrey.betker@ainc-inac.gc.ca	6/21/2011	6/28/2011
Federal	Transport Canada	Environmental Affairs	Regional Manager	Ms. Monique Mousseau	Ms. Mousseau	4900 Yonge St., Suite 300	Toronto	ON	M2N 6A5			monique.mousseau@tc.gc.ca	6/21/2011	6/28/2011
Federal	Transport Canada	Marine Safety	Superintendent	Mr. Barry Putt	Mr. Putt	100 South Front Street	Sarnia	ON	N7T 2M1			barry.putt@tc.gc.ca	6/21/2011	6/28/2011
Federal	Canadian Transportation Agency		Secretariat Directorate	Ms. Cathy Murphy	Ms. Murphy	15 Eddy Street	Gatineau	QC	K1A 0N9			cathy.murphy@ctc-cta.gc.ca	6/21/2011	6/28/2011
Provincial	Ministry of Aboriginal Affairs	Consultation Unit, Aboriginal Relations	Manager	Ms. Heather Levecque	Ms. Levecque	160 Bloor St. E., 9th Flr	Toronto	ON	M7A 2E6	(416)325-4044	(416)326-1066	heather.levecque@ontario.ca	6/21/2011	6/28/2011
Provincial	Ministry of Aboriginal Affairs	Technical	Team Lead	Mr. David Pickles	Mr. Pickles	160 Bloor St. E., 9th Flr	Toronto	ON	M7A 2E6	(416)326-4757		david.pickles@ontario.ca	6/21/2011	6/28/2011
Provincial	Ministry of Aboriginal Affairs			Mr. Martin Rukavina	Mr. Rukavina	160 Bloor St. E., 9th Flr	Toronto	ON	M7A 2E6			martin.rukavina@ontario.ca	6/21/2011	6/28/2011
Provincial	Ministry of Tourism and Culture	Culture Services Unit	A/Heritage Planner, Central and Southeast	Ms. Rosi Zirger	Ms. Zirger	401 Bay St. 17th Floor	Toronto	ON	M7A 0A7	(416)314-7159	(416)314-7175	rosi.zirger@ontario.ca	6/21/2011	6/28/2011
Provincial	Ministry of Environment	Central Region, Halton-Peel District Office	District Manager	Mr. Vincent Sferrazza	Mr. Sferrazza	4145 North Service Rd, Suite 300	Burlington	ON	L7L 6A3			vincent.sferrazza@ontario.ca	6/21/2011	6/28/2011
Provincial	Ministry of Environment	Environmental Assessment and Approvals Branch	Director	Ms. Agatha Garcia-Wright	Ms. Garcia-Wright	2 St. Clair Ave W, Flr 12A	Toronto	ON	M4V 1L5			agatha.garciawright@ontario.ca	6/21/2011	6/28/2011
Provincial	Ministry of Environment	Environmental Assessment and Approvals Branch	EA and Planning Coordinator	Ms. Chunmei Liu	Ms. Liu	5775 Yonge St, 9th Flr	Toronto	ON				chunmei.liu@ontario.ca	6/21/2011	6/28/2011

Provincial	Ministry of Municipal Affairs and Housing	Central Municipal Services Office	Manager Community Planning and Development	Mr. Victor Doyle	Mr. Doyle	777 Bay St., 2nd Flr	Toronto	ON	M5G 2E5	(416)585-6109	(416)585-6882	victor.doyle@ontario.ca	6/21/2011	6/28/2011
Provincial	Ministry of Natural Resources	Aurora District Office	District Planner	Mr. Steve Strong	Mr. Strong	50 Bloomington Rd W, RR#2	Aurora	ON	L4G 2G8			steven.strong@ontario.ca	6/21/2011	6/28/2011
Provincial	Credit Valley Conservation Authority		Manager, Planning Ecology	Mr. Liam Murray	Mr. Murray	1255 Old Derry Rd	Mississauga	ON	L5N 6R4			lmurray@creditvalleycons.com	6/21/2011	6/28/2011
Provincial	Toronto Region Conservation Authority	Planning and Development	Planner II, Environmental Assessment	Ms. Sharon Lingertat	Ms. Lingertat	5 Shoreham Dr	Downsview	ON	M3N 1S4			slingertat@trca.on.ca	6/21/2011	6/28/2011
Provincial	Ministry of Agriculture, Food and Rural Affairs	Central West-Ontario	Rural Planner	Ms. Jackie Van de Valk	Ms. Van De Valk	6484 Wellington Rd 7, Unit 10	Elora	ON	N0B 1S0			jackie.vandevalk@ontario.ca	6/21/2011	6/28/2011
Provincial	Ministry of Energy and Infrastructure	Ontario Realty Corporation, Professional Services	General Manager, Planning, Survey and Appraisal	Mr. Anil Wijesooriya	Mr. Wijesooriya	1 Dundas St. W. Suite 2000	Toronto	ON	M5G 2L5			anil.wijesooriya@ontario.ca	6/21/2011	6/28/2011
Provincial	Ministry of Energy and Infrastructure	Strategic Policy Branch, Conservation & Strategic Policy Division	Sr. Policy Advisor	Mr. Hartley Springman	Mr. Springman	880 Bay St., 6th Flr	Toronto	ON	M7A 2C1	(416)327-7276	(416)327-7204	hartley.springman@ontario.ca	6/21/2011	6/28/2011
Provincial	Ministry of Energy and Infrastructure	Ontario Growth Secretariat	Manager Growth Policy	Mr. Jamie Austin	Mr. Austin	777 Bay St., 4th Flr, Suite 425	Toronto	ON	M5G 2E5	(416)325-5794	(416)325-7403	jamie.austin@ontario.ca	6/21/2011	6/28/2011
Provincial	Ontario Provincial Police	OPP Facilities Section, Accommodation Services Section	Manager	Ms. Sheryl Bennett	Ms. Bennett	777 Memorial Avenue	Orillia	ON	L3V 7V3			sheryl.bennett@ontario.ca	6/21/2011	6/28/2011
Provincial	GO Transit and Metrolinx	Transportation Planning & Development	Manager	Mr. Dan Francey	Mr. Francey	20 Bay St, Suite 600	Toronto	ON	M5J 2W3	(416)869-3600	(416)869-1563	dan.francey@go-transit.com	6/21/2011	6/28/2011
Political	Mississauga-Streetsville		MPP	Mr. Bob Delaney	Mr. Delaney	2000 Argentia Road, Plaza IV, Suite 220	Mississauga	ON	L5N 1W1					
Political	Mississauga-Brampton South		MPP	Ms. Amrit Mangat	Ms. Mangat	7045 Edwards Boulevard, Suite 203	Mississauga	ON	L5S 1X2					
Political	Mississauga -Streetsville		MP	Ms. Eve Adams	Ms. Adams	House of Commons	Ottawa	ON	K1A 0A4					
Political	Mississauga-Brampton South		MP	Mr. Brad Butt	Mr. Butt	154 Queen Street South, Unit 104	Mississauga	ON	L5M 2P4					

Affiliation	Agency	Sub-Agency	Title	Contact Names	Greeting	Street Address	City	Prov	P.C.
Municipality	Region of Peel		Regional Clerk			10 Peel Centre Dr., Suite A, 5th Flr	Brampton	ON	L6T 4B9
Municipality	Region of Peel		Regional Chair	Mr. Emil Kolb	Mr. Kolb	10 Peel Centre Dr., Suite A, 5th Flr	Brampton	ON	L6T 4B9
Municipality	Region of Peel	Public Works	Commissioner	Mr. Dan Labrecque	Mr. Labrecque	10 Peel Centre Dr., Suite A, 4th Flr	Brampton	ON	L6T 4B9
Municipality	Region of Peel	Transportation Division	Director	Mr. Damian Albanese	Mr. Albanese	9445 Airport Rd, 3rd Flr	Brampton	ON	L6S 4J3
Municipality	Region of Peel		Strategist - Infrastructure Planning and Design	Mr. Gary Kocialek, P.Eng.	Mr. Kocialek	10 Peel Centre Dr. 6th Flr	Brampton	ON	L6T 4B9
Municipality	Region of Peel	Transportation Division	Manager of Transportation Systems Planning	Mr. Sabbir Saiyed	Mr. Saiyed	9 Peel Centre Dr. 6th Flr	Brampton	ON	L6T 4B8
Municipality	Region of Peel	Planning	Principal Planner	Ms. Margie Chung	Ms. Chung	10 Peel Centre Dr. 6th Flr	Brampton	ON	L6T 4B9
Municipality	Region of Peel	Ambulance and Emergency Services	Director	Mr. Peter Dundas	Mr. Dundas	5299 Maingate Dr	Mississauga	ON	L4W 1G6
Municipality	Peel Region Police	12 Division	A/Superintendent	Mr. David Downer	Mr. Downer	4600 Dixie Road	Mississauga	ON	L4W 2R1
Municipality	City of Mississauga	Office of the City Clerk	City Clerk	Ms. Crystal Greer	Ms. Greer	300 City Centre Dr, 3rd Flr	Mississauga	ON	L5B 3C1
Municipality	City of Mississauga	Transportation Planning	Manager	Mr. Robert Sasaki	Mr. Sasaki	3484 Sementyk Crt.	Mississauga	ON	L5C 4R1
Municipality	City of Mississauga	Transportation and Works Department	Transportation Project Manager	Mr. Abdul Shaikh	Mr. Shaikh	201 City Centre Dr, Suite 800	Mississauga	ON	L5B 2T4
Municipality	City of Mississauga		Ward 5 Councillor	Ms. Eve Adams	Ms. Adams	300 City Centre Dr.	Mississauga	ON	L5B 3C1
Municipality	City of Mississauga	Fire and Emergency Services	Information Coordinator			15 Fairview Rd W	Mississauga	ON	L5H 1K7
Municipality	City of Mississauga	Fire and Emergency Services	Fire Chief	Mr. John A. McDougall	Mr. McDougall	15 Fairview Rd W	Mississauga	ON	L5B 1K7
Municipality	City of Brampton		Mayor	Susan Fennell	Mayor Fennell	2 Wellington St W	Brampton	ON	L6Y 4R2
Municipality	City of Brampton	Management and Administrative Services	Commissioner	Ms. Kathy Zammit	Ms. Zammit	2 Wellington St W	Brampton	ON	L6Y 4R2
Municipality	City of Brampton	Works and Transportation	Commissioner	Mr. John Corbett	Mr. Corbett	2 Wellington St W	Brampton	ON	L6Y 4R2
Municipality	City of Brampton	Brampton Fire and Emergency Services	Fire Chief	Mr. Andy MacDonald	Mr. MacDonald	8 Rutherford Rd S	Brampton	ON	L6W 3J1

Municipality	City of Brampton	City Clerk's Office	City Clerk			2 Wellington St W	Brampton	ON	L6Y 4R2
Other	Peel District School Board	Planning and Accommodation Department	Intermediate Planning Officer	Mr. Paul Mountford	Mr. Mountford	HJA Brown Education Centre, 5650 Hurontario St	Mississauga	ON	L5R 1C6
Other	Dufferin-Peel Catholic District School Board	Planning Department	Manager of Planning	Mr. Thane Munn	Mr. Munn	40 Matheson Blvd W	Mississauga	ON	L5R 1C5
Other	TransHelp	Advisory Committee				3190 Mavis Road	Mississauga	ON	L5C 1T9
Other	Mississauga Board of Trade		Chair	Mr. Ian MacFadden	Mr. MacFadden	701-77 City Centre Drive	Mississauga	ON	L5C 1T9
Utilities	Bell Canada			Mr. Rick Englehardt	Mr. Englehardt	2 Fieldway Dr. 9th Flr	Etobicoke	ON	
Utilities	Enbridge Gas Distribution Inc.	Special Projects	Planning and Design Analyst	Mr. Martin Paquette	Mr. Paquette	500 Consumers Rd, 4th Flr, Post D1	North York	ON	
Utilities	Rogers Cable	OPE-GTA West	Planner	Mr. Corry Baker	Mr. Baker	3573 Wolfedale Rd	Mississauga	ON	
Utilities	Hydro One Telecom		OSP Manager	Mr. Ian Mitchell	Mr. Mitchell	65 Kelfield St.	Rexdale	ON	
Utilities	Hydro One Networks	Area Distribution	Engineering Technician	Mr. B. Jamie Bignell	Mr. Bignell	913 Crawford Dr.	Peterborough	ON	
Utilities	Trans Northern Pipelines		Coordinator	Mr. Satish Kumar	Mr. Kumar	45 Vogell Rd., Suite 310	Richmond Hill	ON	
Utilities	Peel Region	PUCC/Permits	Information Services, Operations Support, Public Works	Ms. Wendy Jawdek	Ms. Jawdek	9445 Airport Road, 3rd Floor	Brampton	ON	
Utilities	Greater Toronto Airport Authority	Strategic Planning and Airport Development	Geomatics Planning Analyst	Ms. Agnes Kotowicz	Ms. Kotowicz	PO Box 6031, 3111 Convair Dr	Toronto AMF	ON	
Utilities	Orangeville-Brampton Railway		Operations Manager	Mr. Steve Gallagher	Mr. Gallagher	75 1st St	Orangeville	ON	

Affiliation	Title	Contact Names	Greeting	Street Address	City	Prov	P.C.
Union of Ontario Indians, Nipissing First Nation	CEO	Mr. Alan Ozawanimke	Mr. Ozawanimke	PO Box 711	North Bay	ON	P1B 8J8
Association of Iroquois and Allied Indians	Director of Intergovernmental Affairs	Ms. Adrianna Poulette	Ms. Poulette	387 Princess Street	London	ON	N6B 2A7
Coordinator for the Williams Treaties	Barrister/Solicitor	Karry Sandy-McKenzie	Ms. Sandy	8 Creswick Court	Barrie	ON	L4M 2J7
Mississauga of Scugog Island First Nation	Chief	Tracy Gauthier / Rhonda Coppaway	Chief Gauthier	RR#5, 22521 Island Road	Port Perry	ON	L9L 1B6
Chippewas of Georgina Island First Nation	Chief	Donna Big Canoe	Chief Big Canoe	RR#2, PO Box N13	Sutton West	ON	L0E 1R0
Chippewas of Mnjikaning First Nation	Chief	Sharon Stinson Henry	Chief Stinson Henry	5884 Rama Road, Suite 200	Rama	ON	L0K 1T0
Hiawatha First Nation	Chief	Laurie Carr	Chief Carr	123 Paudash St	Hiawatha	ON	K0L 2G0
Hiawatha First Nation	Councillor	Lorne Paudash	Mr. Paudash	123 Paudash St	Hiawatha	ON	K0L 2G0
Beausoleil First Nation	Chief	Rodney Monague Jr.	Chief Monague Jr.	1 Ogema St	Christian Island	ON	L0K 1C0
Alderville First Nation	Chief	James Marsden	Chief Marsden	Alderville Administrative Office, 11696 Second Line Rd., PO Box 46	Alderville	ON	K0K 2X0
Curve Lake First Nation	Chief	Keith Knott	Chief Knott	22 Winookeeda Rd	Curve Lake	ON	K0L 1R0
Curve Lake First Nation	Councillor	Ted Coppaway	Mr. Coppaway	22 Winookeeda Rd	Curve Lake	ON	K0L 1R0
Mississaugas of the New Credit First Nations	Chief	Bryan Laforme	Chief Laforme	2789 Mississauga Rd, RR#6	Hagersville	ON	N0A 1H0
Mississauga of the New Credit First Nations		Margaret Sault	Ms. Sault	2789 Mississauga Rd, RR#6	Hagersville	ON	N0A 1H0
Six Nations Haudensaunee Confederacy Council	Chief	Allen McNaughton c/o Leroy Hill	Chief McNaughton	RR #2	Ohsweken	ON	N0A 1M0
Six Nations of the Grand River Territory	Chief	William Montour	Chief Montour	1695 Chiefswood Road, PO Box 5000	Ohsweken	ON	N0A 1M0
Metis Consultation Unit				Head Office, 500 Old St. Patrick St, Box 5000	Ohsweken	ON	K1N 9G4
Credit River Metis Council				c/o 1515 Matheson Blvd. E. #103	Mississauga	ON	L4W 2P5



AECOM
300 Water Street
Whitby, ON, Canada L1N 9J2
www.aecom.com

905 668 9363 tel
905 668 0221 fax

June 24, 2011

Contact Name
Contact Title
Company Name
Company Address

Dear Contact Name:

**Regarding: Notice of Study Commencement
Highway 401 Widening – Highway 403/410 Interchange to the Credit River
Group Work Project (G.W.P.) 2150-01-00
Detail Design**

AECOM Canada Ltd. (AECOM) has been retained by the Ontario Ministry of Transportation (MTO) to undertake the Detail Design Project for the ultimate widening of Highway 401 from the Highway 403/410 interchange to the Credit River in the City of Mississauga, Regional Municipality of Peel, a distance of 7 km. The project will complete the expansion of Highway 401 from Highway 410/403 interchange to the Credit River, from its current 6 lanes to a 12-lane core/collector system including a High Occupancy Vehicle (HOV) lane in each direction. The proposed improvements include:

- Widening of Highway 401 to a 12-lane core/collector system from west of Hurontario Street to the Credit River;
- Construction of HOV lanes in both directions;
- Reconstruction or extension of the Mavis Road interchange;
- Removal of the 2nd Line West structure;
- Culvert extensions at Fletcher's Creek; and
- Expansion of COMPASS system

The purpose of this letter is to inform you of the project and to solicit comments that should be addressed throughout the detail design stage. To this end, your assistance in ensuring this letter is circulated to the appropriate personnel within your organization or agency is greatly appreciated.

This project is subject to the *Environmental Assessment Act* and has followed the environmental planning process for Group "B" Projects as described in the *Class Environmental Assessment for Provincial Transportation Facilities* (2000), with public input throughout. A Consultation Plan has been developed to ensure external agencies and the public have an opportunity to participate in the planning and decision-making processes throughout the project. Please find enclosed a copy of the Notice of Study Commencement providing further details regarding the project.

We would appreciate receiving any information your agency may have which is relevant to this project. If your agency has any concerns and/or comments regarding this project and you wish to provide input, please contact the undersigned using the **Reply Form** provided. Should the proposed project have no affect on your agency's program mandate and/or policies, please advise the

undersigned of this fact by returning the **Reply Form** provided. Your response is appreciated by **July 18, 2011** so that we can meet the study schedule, and ensure your issues/concerns are addressed in a timely manner.

If you require additional information or wish to discuss the project or the study process, please contact either one of the following Project Team members listed in the enclosed Notice. Project information will be posted and updated on the project website: **www.401expansion-mississauga.ca**.

Thank you for your assistance with this project.

Sincerely,
AECOM Canada Ltd.



Brian Ruck, P.Eng.
Consultant Project Manager
Brian.Ruck@aecom.com

CL:mo
Encl.
cc: Miao Zhou, P.Eng. – MTO Senior Project Engineer
Larry Sarris – MTO Environmental Planner
Mirjana Osojnicki – AECOM Environmental Planner

**Ministry of
Transportation**
Engineering Office
Central Region
4th Floor, Bldg. D
1201 Wilson Avenue
Downsview, Ontario M3M 1J8
Fax: (416) 235-3576

**Ministère des
Transports**
Bureau de génie
Région du Centre
4e étage, édifice D
1201 avenue Wilson
Downsview Ontario M3M1J8
Télé: (416) 235-3576



July 4, 2011

<FIRST NATION BAND LETTER>

**Re: Notice of Study Commencement
Highway 401 Widening – Highway 403/410 Interchange to the Credit River
Group Work Project (G.W.P.) 2150-01-00
Detail Design**

AECOM Canada Ltd. (AECOM) has been retained by the Ontario Ministry of Transportation (MTO) to undertake the Detail Design Project for the ultimate widening of Highway 401 from the Highway 403/410 interchange to the Credit River in the City of Mississauga, Regional Municipality of Peel, a distance of 7 km. The project will complete the expansion of Highway 401 from Highway 410/403 interchange to the Credit River, from its current 6 lanes to a 12-lane core/collector system including a High Occupancy Vehicle (HOV) lane in each direction. The proposed improvements include:

- Widening of Highway 401 to a 12-lane core/collector system from west of Hurontario Street to the Credit River;
- Construction of HOV lanes in both directions;
- Reconstruction or extension of the Mavis Road interchange;
- Removal of the 2nd Line West structure;
- Culvert extensions at Fletcher's Creek; and
Expansion of COMPASS system.

The purpose of this letter is to inform you of the Study and to solicit comments that should be addressed throughout the detail design stage. To this end, your assistance in ensuring this letter is circulated to the appropriate personnel within your organization or agency is greatly appreciated.

This Study is subject to the *Environmental Assessment Act* and has followed the environmental planning process for Group "B" Projects as described in the *Class Environmental Assessment for Provincial Transportation Facilities* (2000), with public input throughout. A Consultation Plan has been developed to ensure external agencies and the public have an opportunity to participate in the planning and decision-making processes throughout the Study. Please find enclosed a copy of the Notice of Study Commencement providing further details regarding the project.

We would appreciate receiving any information your agency may have which is relevant to this Study. If your agency/First Nation has any concerns and/or comments regarding this Study and you wish to provide input, please contact the undersigned. Your response is appreciated by **July 25, 2011** so that we can meet the study schedule, and ensure your issues/concerns are addressed in a timely manner.

If you require additional information or wish to discuss the project or the study process, please contact the undersigned. Study information will be posted and updated on the project website: **www.401expansion-mississauga.ca**.

Thank-you for your assistance with this project.

Sincerely,

ORIGINAL SIGNED

Miao Zhou, M. Eng., P. Eng.
Senior Project Engineer
Ministry of Transportation, Central Region

Encl.

Information will be collected in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.



Ministry of
Transportation
Engineering Office
Central Region
4th Floor, Bldg. D
1201 Wilson Avenue
Downsview, Ontario M3M 1J8
Fax: (416) 235-3576

Ministère des
Transports
Bureau de génie
Région du Centre
4e étage, édifice D
1201 avenue Wilson
Downsview Ontario M3M1J8
Télé: (416) 235-3576

July 4, 2011

Mr. Brad Butt, MP
154 Queen Street South, Unit 104
Mississauga, ON L5M 2P4

Dear Mr. Butt,

**Re: Notice of Study Commencement
Highway 401 Widening – Highway 403/410 Interchange to the Credit River
Group Work Project (G.W.P.) 2150-01-00
Detailed Design**

AECOM Canada Ltd. (AECOM) has been retained by the Ontario Ministry of Transportation (MTO) to undertake the Detail Design Project for the ultimate widening of Highway 401 from the Highway 403/410 interchange to the Credit River in the City of Mississauga, Regional Municipality of Peel, a distance of 7 km. The project will complete the expansion of Highway 401 from Highway 410/403 interchange to the Credit River, from its current 6 lanes to a 12-lane core/collector system including a High Occupancy Vehicle (HOV) lane in each direction. The proposed improvements include:

- Widening of Highway 401 to a 12-lane core/collector system from west of Hurontario Street to the Credit River;
- Construction of HOV lanes in both directions;
- Reconstruction or extension of the Mavis Road interchange;
- Removal of the 2nd Line West structure;
- Culvert extensions at Fletcher's Creek; and
- Expansion of COMPASS system

The purpose of this letter is to inform you of the project and to solicit comments that should be addressed throughout the detail design stage. This project is subject to the Environmental Assessment Act and has followed the environmental planning process for Group "B" Projects as described in the *Class Environmental Assessment for Provincial Transportation Facilities (2000)*, with public input throughout. A Consultation Plan has been developed to ensure external agencies and the public have an opportunity to participate in the planning and decision-making processes throughout the project.

Please find enclosed a copy of the Notice of Study Commencement providing further details regarding the project. The Notice was published in the *Mississauga News* and the *Toronto Star* during the week of June 20th and in the *L'Express de Toronto* during the week of June 27th, 2011.

If you require additional information or wish to discuss the project or the study process, please contact either one of the following Project Team members listed in the enclosed Notice. Project information will be posted and updated on the project website: **www.401expansion-mississauga.ca**.

Thank you for your assistance with this project.

Sincerely,



Miao Zhou, M.Eng. P.Eng.
Senior Project Engineer
Ontario Ministry of Transportation

CL:mo
Encl.

cc: Brian Ruck – AECOM Project Manager
Larry Sarris – MTO Environmental Planner
Mirjana Osojnicky – AECOM Environmental Planner

Ministry of
Transportation
Engineering Office
Central Region
4th Floor, Bldg. D
1201 Wilson Avenue
Downsview, Ontario M3M 1J8
Fax: (416) 235-3576

Ministère des
Transports
Bureau de génie
Région du Centre
4e étage, édifice D
1201 avenue Wilson
Downsview Ontario M3M1J8
Télé: (416) 235-3576



July 4, 2011

Amrit Mangat, MPP
7045 Edwards Boulevard, Suite 203
Mississauga, Ontario L5S 1X2

Dear Ms. Mangat,

**Re: Notice of Study Commencement
Highway 401 Widening – Highway 403/410 Interchange to the Credit River
Group Work Project (G.W.P.) 2150-01-00
Detailed Design**

AECOM Canada Ltd. (AECOM) has been retained by the Ontario Ministry of Transportation (MTO) to undertake the Detail Design Project for the ultimate widening of Highway 401 from the Highway 403/410 interchange to the Credit River in the City of Mississauga, Regional Municipality of Peel, a distance of 7 km. The project will complete the expansion of Highway 401 from Highway 410/403 interchange to the Credit River, from its current 6 lanes to a 12-lane core/collector system including a High Occupancy Vehicle (HOV) lane in each direction. The proposed improvements include:

- Widening of Highway 401 to a 12-lane core/collector system from west of Hurontario Street to the Credit River;
- Construction of HOV lanes in both directions;
- Reconstruction or extension of the Mavis Road interchange;
- Removal of the 2nd Line West structure;
- Culvert extensions at Fletcher's Creek; and
- Expansion of COMPASS system

The purpose of this letter is to inform you of the project and to solicit comments that should be addressed throughout the detail design stage. This project is subject to the Environmental Assessment Act and has followed the environmental planning process for Group "B" Projects as described in the *Class Environmental Assessment for Provincial Transportation Facilities (2000)*, with public input throughout. A Consultation Plan has been developed to ensure external agencies and the public have an opportunity to participate in the planning and decision-making processes throughout the project.

Please find enclosed a copy of the Notice of Study Commencement providing further details regarding the project. The Notice was published in the *Mississauga News* and the *Toronto Star* during the week of June 20th and in the *L'Express de Toronto* during the week of June 27th, 2011.

If you require additional information or wish to discuss the project or the study process, please contact either one of the following Project Team members listed in the enclosed Notice. Project information will be posted and updated on the project website:
www.401expansion-mississauga.ca.

Thank you for your assistance with this project.

Sincerely,



Miao Zhou, M.Eng. P.Eng.
Senior Project Engineer
Ontario Ministry of Transportation

CL:mo
Encl.

cc: Brian Ruck – AECOM Project Manager
Larry Sarris – MTO Environmental Planner
Mirjana Osojnicki – AECOM Environmental Planner

Ministry of
Transportation
Engineering Office
Central Region
4th Floor, Bldg. D
1201 Wilson Avenue
Downsview, Ontario M3M 1J8
Fax: (416) 235-3576

Ministère des
Transports
Bureau de génie
Région du Centre
4e étage, édifice D
1201 avenue Wilson
Downsview Ontario M3M1J8
Télé: (416) 235-3576



July 4, 2011

Ms. Eve Adams, MP
4870 Tomken Road, Unit 7
Mississauga, Ontario
L4W 1J8

Dear Ms. Adams,

**Re: Notice of Study Commencement
Highway 401 Widening – Highway 403/410 Interchange to the Credit River
Group Work Project (G.W.P.) 2150-01-00
Detailed Design**

AECOM Canada Ltd. (AECOM) has been retained by the Ontario Ministry of Transportation (MTO) to undertake the Detail Design Project for the ultimate widening of Highway 401 from the Highway 403/410 interchange to the Credit River in the City of Mississauga, Regional Municipality of Peel, a distance of 7 km. The project will complete the expansion of Highway 401 from Highway 410/403 interchange to the Credit River, from its current 6 lanes to a 12-lane core/collector system including a High Occupancy Vehicle (HOV) lane in each direction. The proposed improvements include:

- Widening of Highway 401 to a 12-lane core/collector system from west of Hurontario Street to the Credit River;
- Construction of HOV lanes in both directions;
- Reconstruction or extension of the Mavis Road interchange;
- Removal of the 2nd Line West structure;
- Culvert extensions at Fletcher's Creek; and
- Expansion of COMPASS system

The purpose of this letter is to inform you of the project and to solicit comments that should be addressed throughout the detail design stage. This project is subject to the Environmental Assessment Act and has followed the environmental planning process for Group "B" Projects as described in the *Class Environmental Assessment for Provincial Transportation Facilities (2000)*, with public input throughout. A Consultation Plan has been developed to ensure external agencies and the public have an opportunity to participate in the planning and decision-making processes throughout the project.

Please find enclosed a copy of the Notice of Study Commencement providing further details regarding the project. The Notice was published in the *Mississauga News* and the *Toronto Star* during the week of June 20th and in the *L'Express de Toronto* during the week of June 27th, 2011.

If you require additional information or wish to discuss the project or the study process, please contact either one of the following Project Team members listed in the enclosed Notice. Project information will be posted and updated on the project website: **www.401expansion-mississauga.ca**.

Thank you for your assistance with this project.

Sincerely,



Miao Zhou, M.Eng. P.Eng.
Senior Project Engineer
Ontario Ministry of Transportation

CL:mo
Encl.

cc: Brian Ruck – AECOM Project Manager
Larry Sarris – MTO Environmental Planner
Mirjana Osojnicki – AECOM Environmental Planner

Ministry of
Transportation
Engineering Office
Central Region
4th Floor, Bldg. D
1201 Wilson Avenue
Downsview, Ontario M3M 1J8
Fax: (416) 235-3576

Ministère des
Transports
Bureau de génie
Région du Centre
4e étage, édifice D
1201 avenue Wilson
Downsview Ontario M3M1J8
Télé: (416) 235-3576



July 4, 2011

Mr. Bob Delaney, MPP
Meadowvale Corporate Centre
Suite 220, 2nd Floor, Plaza IV
2000 Argentia Road
Mississauga, Ontario L5N 1W1

Dear Mr. Delaney,

**Re: Notice of Study Commencement
Highway 401 Widening – Highway 403/410 Interchange to the Credit River
Group Work Project (G.W.P.) 2150-01-00
Detailed Design**

AECOM Canada Ltd. (AECOM) has been retained by the Ontario Ministry of Transportation (MTO) to undertake the Detail Design Project for the ultimate widening of Highway 401 from the Highway 403/410 interchange to the Credit River in the City of Mississauga, Regional Municipality of Peel, a distance of 7 km. The project will complete the expansion of Highway 401 from Highway 410/403 interchange to the Credit River, from its current 6 lanes to a 12-lane core/collector system including a High Occupancy Vehicle (HOV) lane in each direction. The proposed improvements include:

- Widening of Highway 401 to a 12-lane core/collector system from west of Hurontario Street to the Credit River;
- Construction of HOV lanes in both directions;
- Reconstruction or extension of the Mavis Road interchange;
- Removal of the 2nd Line West structure;
- Culvert extensions at Fletcher's Creek; and
- Expansion of COMPASS system

The purpose of this letter is to inform you of the project and to solicit comments that should be addressed throughout the detail design stage. This project is subject to the Environmental Assessment Act and has followed the environmental planning process for Group "B" Projects as described in the *Class Environmental Assessment for Provincial Transportation Facilities (2000)*, with public input throughout. A Consultation Plan has been developed to ensure external agencies and the public have an opportunity to participate in the planning and decision-making processes throughout the project.

Please find enclosed a copy of the Notice of Study Commencement providing further details regarding the project. The Notice was published in the *Mississauga News* and the

Toronto Star during the week of June 20th and in the *L'Express de Toronto* during the week of June 27th, 2011.

If you require additional information or wish to discuss the project or the study process, please contact either one of the following Project Team members listed in the enclosed Notice. Project information will be posted and updated on the project website:

www.401expansion-mississauga.ca.

Thank you for your assistance with this project.

Sincerely,



Miao Zhou, M.Eng. P.Eng.
Senior Project Engineer
Ontario Ministry of Transportation

CL:mo
Encl.

cc: Brian Ruck – AECOM Project Manager
Larry Sarris – MTO Environmental Planner
Mirjana Osojnicky – AECOM Environmental Planner

REPLY FORM**PLEASE REPLY BY July 18, 2011****To:** MIRJANA OSOJNICKI, AECOM**Date:** _____**Fax:** (905) 238-0038**E-mail:** mirjana.osojnicki@aecom.com**Re:** Notice of Study Commencement Highway 401 Widening, from Highway 403/410 Interchange to the Credit River, City of Mississauga, Regional Municipality of Peel, 7 km, GWP 2150-01-00

NAME: _____ **Use my contact information as the key project contact for future correspondence****TITLE:** _____**MUNICIPALITY/AGENCY:** _____**ADDRESS:** _____**POSTAL CODE:** _____**PHONE:** _____**FAX:** _____**E-MAIL:** _____**Please indicate the appropriate response:**

- My group/agency **is interested** in providing input regarding this Study. Please leave me on the Study Mailing List.
- My group/agency **is not interested** in providing input regarding this Study but would like to be kept informed. Please leave me on the Study Mailing List.
- Please **remove** my group/agency from Study Mailing List.

Area of interest or concern/preliminary comments:



Project Website

HIGHWAY 401 WIDENING

FROM HIGHWAY 403/410 INTERCHANGE TO THE CREDIT RIVER

AECOM

Detail Design G.W.P. 2150-01-00

CONTACT US | SITE MAP



HOME

EA PROCESS

PROJECT OVERVIEW

BACKGROUND

CONSULTATION/
UPCOMING EVENTS

REPORTS

FAQs

LINKS

CONTACT US



PROJECT OVERVIEW

Highway 401 is recognized as a key economic corridor. It provides access within the Greater Toronto Area, across Ontario and to the U.S.A. through border crossings at Windsor and Samia. In 2005, Highway 401 carried a commodity value of \$ 900 M per day through the project limits. Safe and efficient goods movement plays an important role in this transportation corridor.

The Ministry is currently undertaking a number of studies to examine broader transportation initiatives such as transit and high occupancy vehicle facilities. This included the Class Environmental Assessment (EA) Study for Improvements to Highway 401 from east of the Credit River to east of Trafalgar Road (9.7km). The purpose of this study was to identify the interim and ultimate capacity for this section of Highway 401 and to consider provisions for high occupancy vehicle lanes (HOV) and transit.

AECOM Canada Ltd (AECOM) has been retained by the Ontario Ministry of Transportation (MTO) to undertake the Detail Design project for the widening of Highway 401 from the Highway 403/410 Interchange to the Credit River in the City of Mississauga, Regional Municipality of Peel, a distance of 7 km.

The project section of Highway 401 is currently a 6 lane divided freeway. East of Highway 410/403, a 12-lane express collector system is in place. A portion of Highway 401 is currently under construction for widening to a 12-lane core/collector system from Highway 410/403 to west of Hurontario Street, including reconstruction of the Hurontario Interchange (Contract #2009-2031).

This project will carry forward the issues and commitments identified as part of the August 2005 Transportation Environmental Study Report (TESR) (Approved, 2007), which identified design improvements for this section of Highway 401. This includes:

- Widening of Highway 401 to a 12-lane core/collector system from west of Hurontario Street to the Credit River;
- Construction of HOV lanes in both directions;
- Reconstruction or extension of the Mavis Road Interchange;
- Removal of the 2nd Line West structure;
- Culvert extensions at Fletcher's Creek; and
- Expansion of COMPASS system

A key component of addressing these issues is through consultation. Further information about the consultation program is in the [Upcoming Events](#) section.



Please Note: In some cases you will require Adobe Acrobat Reader to view these files. Click [here](#) on the link below to download this software.

HOME | EA PROCESS | PROJECT OVERVIEW | BACKGROUND | UPCOMING EVENTS | REPORTS | LINKS | CONTACT US

COPYRIGHT AECOM 2011 | EMAIL WEBMASTER



Public Information Centre

NOTICE OF PUBLIC INFORMATION CENTRE

Highway 401 Widening From Highway 403/410 Interchange to the Credit River G.W.P.s 2150-01-00 and 2152-01-00

THE PROJECT

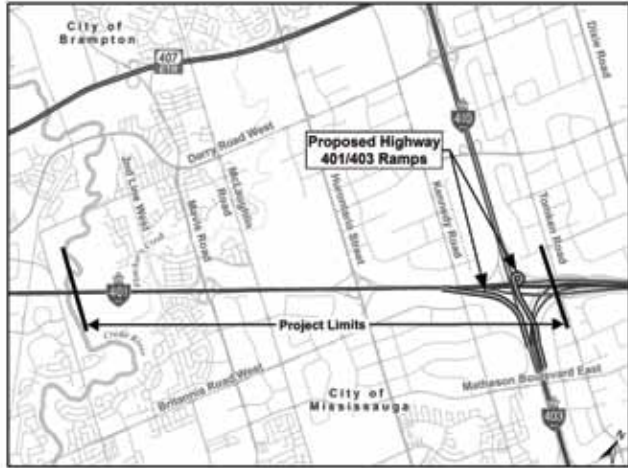
AECOM Canada Ltd. is undertaking the Detail Design on behalf of the Ontario **Ministry of Transportation (MTO)** for the ultimate widening of Highway 401 from the Highway 403/410 interchange to the Credit River, Mississauga, a distance of 7 km (see key plan). This project involves:

Under G.W.P. 2150-01-00

- Expansion of Highway 401 from its current six lanes to a 12-lane core/collector system including High Occupancy Vehicle (HOV) lanes;
- Removal of the Second Line West structure over Highway 401 to accommodate the widening; and
- Construction of two ramps from Highway 401 eastbound to Highway 403 southbound and from Highway 403 northbound to Highway 401 westbound at the 401/410/403 interchange.

Under G.W.P. 2152-01-00

- Extension of the Mavis Road bridge as an advanced construction contract.



THE PROCESS

This project will follow the environmental planning process for Group 'B' undertakings as defined in the *Class Environmental Assessment for Provincial Transportation Facilities (2000)*. A previous Transportation Environmental Study Report (TESR) was prepared in August 2005 and received environmental approval. Since that time, it has been determined that reconstruction of the Mavis Road bridge can be advanced by preparing the Detail Design separately and documenting this in its own Design and Construction Report (DCR).

A second DCR will subsequently be prepared to document the Detail Design for the remainder of the highway expansion work. A TESR Addendum will be prepared for the proposed extension of Belgrave Road to connect to Mavis Road. Details regarding the release of these documents will be provided in future notices.

Project information will be posted and updated on the project website: www.401expansion-mississauga.ca.

The City of Mississauga is considering a separate study for pedestrian/cycling options across Highway 401 at the location of the removed Second Line West structure. This study will be undertaken in accordance with the *Municipal Class Environmental Assessment* process.

PUBLIC INFORMATION CENTRE

The public is invited to attend a Public Information Centre (PIC) for this project. The purpose of the PIC is to present and receive input on the design details of the proposed Highway 401 improvements, including information on construction staging, potential environmental impacts and proposed mitigation measures. The PIC will be held on:

Date: Wednesday, November 13, 2013
Location: Meadowvale Village Hall
6970 Second Line West, Mississauga, ON
Time: 4:00 p.m. to 8:00 p.m.

This PIC will be an informal drop-in centre. Representatives from the MTO and the project's consultant team will be available to answer questions, receive comments and discuss the next steps in this project. You are encouraged to attend the PIC and provide the Project Team with your views and comments so that they can be considered as the project progresses. For further information or if you wish to be added to the project mailing list, please contact:

Miao Zhou, P.Eng., M.Eng.
Project Engineer
Ontario Ministry of Transportation
Central Region
1201 Wilson Avenue, Building D, 4th Floor
Downsview, ON M3M 1J8
tel: 416-235-4277
fax: 416-235-3576
e-mail: miao.zhou@ontario.ca

Brian Ruck, P.Eng., C.V.S.
Consultant Project Manager
AECOM Canada Ltd.
300 Water Street
Whitby, ON L1N 9J2
toll-free: 1-800-668-1983
tel: 905-668-4021 ext. 2250
fax: 905-665-4867
e-mail: brian.ruck@aecom.com

Comments and information regarding this project are being collected to assist in meeting the requirements of the *Environmental Assessment Act*, in accordance with the *Freedom of Information Act*. This material will be maintained on file for use during the project and may be included in project documentation. With the exception of personal information, all comments will become part of the public record.



MINISTRY OF TRANSPORTATION
PN-5596-URS_ENG
Toronto Star 6C (6.267) x 145ag (set-size)
Mississauga News 6C (6.191") x 150ag

AVIS CONCERNANT LA SÉANCE D'INFORMATION PUBLIQUE

Élargissement de l'autoroute 401 à partir de l'échangeur entre les autoroutes 403 et 410 jusqu'à la rivière Credit G.W.P. 2150-01-00 et G.W.P. 2152-01-00

PROJET

AECOM Canada Ltd. entreprend une étude de conception détaillée pour le compte du **ministère des Transports de l'Ontario (MTO)** en vue du dernier élargissement de l'autoroute 401 à partir de l'échangeur entre les autoroutes 403 et 410 jusqu'à la rivière Credit à Mississauga sur une distance de 7 km (voir la carte-index). Le projet comprend ce qui suit :

Aux termes de G.W.P. 2150-01-00

- Élargissement de l'autoroute 401 qui comporte actuellement 6 voies afin d'y aménager un système de 12 voies principales et collectrices comprenant des voies réservées aux véhicules multioccupants (VMO);
- Enlèvement de la structure de Second Line West au-dessus de l'autoroute 401 afin de permettre l'élargissement;
- Construction de deux bretelles de l'autoroute 401 en direction est jusqu'à l'autoroute 403 en direction sud et de l'autoroute 403 en direction nord jusqu'à l'autoroute 401 en direction ouest à l'échangeur 401/410/403.



Aux termes de G.W.P. 2152-01-00

- Prolongement du pont de Mavis Road à titre de contrat de construction avancée.

PROCESSUS

Ce projet suit le processus de planification environnementale approuvé pour les projets du groupe B en vertu de l'*Évaluation environnementale de portée générale pour les installations provinciales de transport* (2000). Un rapport d'étude environnementale pour les transports (REET) a été préparé en août 2005 et a fait l'objet d'une approbation environnementale. Depuis, il a été déterminé que la reconstruction du pont de Mavis Road pouvait être avancée en préparant une étude de conception détaillée distincte et en documentant ce projet dans un rapport de conception-construction (RCC) s'y rattachant en propre.

Un deuxième RCC a par la suite été préparé pour documenter la conception détaillée du reste des travaux d'élargissement de l'autoroute. Un addenda au REET sera préparé pour le prolongement proposé du Belgrave Road pour le relier au Mavis Road. Les détails concernant la publication de ces documents seront fournis dans des avis futurs.

Des renseignements sur le projet seront fournis et mis à jour sur le site Web du projet : www.401expansion-mississauga.ca.

La Ville de Mississauga envisage de commander une étude distincte visant des options pour la circulation des piétons et des cyclistes au-dessus de l'autoroute 401 à l'endroit où se trouvait la structure enlevée à la Second Line West. Cette étude sera menée conformément au processus de l'*évaluation environnementale municipale de portée générale*.

SÉANCE D'INFORMATION PUBLIQUE

Le public est invité à participer à une séance d'information publique (SIP) pour ce projet. L'objectif de la SIP est de présenter les détails de la conception et de recueillir les commentaires relatifs aux améliorations proposées à l'autoroute 401, ce qui comprend des renseignements sur les étapes de la construction, les incidences environnementales potentielles ainsi que les mesures d'atténuation proposées. Date de la SIP :

Le mercredi 13 novembre 2013
Meadowvale Village Hall, 6970 Second Line West, Mississauga, ON
16 h – 20 h

Cette SIP sera une séance informelle à laquelle tous pourront se présenter à l'heure désirée. Des représentants du MTO et de l'équipe de l'expert-conseil du projet seront disponibles pour répondre aux questions, recevoir des commentaires et discuter des prochaines étapes du projet. Nous vous encourageons à y assister et à exprimer à l'équipe du projet vos opinions et commentaires pour qu'ils puissent être pris en considération au fur et à mesure de l'avancement du projet. Pour en savoir plus ou pour faire ajouter votre nom sur la liste d'envoi du projet, veuillez communiquer avec :

Miao Zhou, P.Eng., M.Eng.
Ingénieur chargé de projet
Ministère des Transports de l'Ontario
Région du Centre
Immeuble D, 4^e étage, 1201, avenue Wilson
Downsview, ON M3M 1J8
Téléphone : 416-235-4277
Télécopieur : 416-235-3576
Courriel : miao.zhou@ontario.ca

Brian Ruck, P.Eng. C.V.S.,
Chargé de projet de l'expert-conseil
AECOM Canada Ltd.
300, rue Water
Whitby, ON L1N 9J2
Sans frais : 1-800-668-1983 ou
Téléphone : 905-668-4021, poste 2250
Télécopieur : 905-665-4867
Courriel : brian.ruck@aecom.com

Les commentaires et les renseignements concernant ce projet sont recueillis afin d'aider à satisfaire aux exigences de la Loi sur les évaluations environnementales et ils seront utilisés conformément à la *Loi sur l'accès à l'information et la protection de la vie privée*. Ces éléments seront conservés dans le dossier pour être utilisés pendant le projet et pourront être inclus dans la documentation du projet. À l'exception des renseignements personnels, tous les commentaires feront partie du domaine public.





AECOM
300 Water Street
Whitby, ON, Canada L1N 9J2
www.aecom.com

905 668 9363 tel
905 668 0221 fax

<<date>>

Sample Letter to municipalities,
agencies and interest groups

Contact Name
Contact Title
Company Name
Company Address

Dear Contact Name:

**Regarding: Notice of Public Information Centre (PIC)
Highway 401 Widening – Highway 403/410 Interchange to the Credit River
Detail Design Group Work Projects (G.W.P. 2150-01-00 and G.W.P. 2152-01-00)**

AECOM Canada Ltd. (AECOM) has been retained by the Ontario Ministry of Transportation (MTO) to undertake the Detail Design for the ultimate widening of Highway 401 from the Highway 403/410 interchange to the Credit River in the City of Mississauga, Regional Municipality of Peel, a distance of 7 km. The project will complete the expansion of Highway 401 from Highway 410/403 interchange to the Credit River, from its current 6 lanes to a 12-lane core/collector system.

The purpose of this letter is to advise you of the upcoming Public Information Centre (PIC) scheduled for **Wednesday November 13, 2013**, and to invite you to attend the session held from **3:00 p.m. to 4:00 p.m.** at the **Meadowvale Village Hall (6970 Second Line West, Mississauga, ON)** for municipal councillors/staff, First Nations representatives and external agency staff only. Additional information is enclosed and available on the project website at www.401expansion-mississauga.ca.

The PIC will be a drop-in style open house format, at which the Recommended Plan will be presented. Staff from Project Team will be in attendance at the PIC to answer questions. You are encouraged to attend the PIC and provide your views and comments so they can be considered.

If you require additional information or wish to discuss the project, please contact either one of the following Project Team members listed in the enclosed Notice.

Sincerely,
AECOM Canada Ltd.

Brian Ruck, P.Eng. CVS
Consultant Project Manager
Brian.Ruck@aecom.com

CL:mo
Encl. Notice of Public Information Centre
cc: Miao Zhou – MTO Project Engineer; Larry Sarris – MTO Environmental Planner
Mirjana Osojnicky – AECOM Environmental Planner



AECOM
300 Water Street
Whitby, ON, Canada L1N 9J2
www.aecom.com

905 668 9363 tel
905 668 0221 fax

Sample Letter to Impacted
Property Owners

<<date>>

Contact Name
Address

Dear Contact Name:

**Regarding: Notice of Public Information Centre (PIC)
Highway 401 Widening – Highway 403/410 Interchange to the Credit River
Detail Design Group Work Projects (G.W.P. 2150-01-00 and G.W.P. 2152-01-00)**

AECOM Canada Ltd. (AECOM) has been retained by the Ontario Ministry of Transportation (MTO) to undertake the Detail Design for the ultimate widening of Highway 401 from the Highway 403/410 interchange to the Credit River in the City of Mississauga, Regional Municipality of Peel, a distance of 7 km. The project will complete the expansion of Highway 401 from Highway 410/403 interchange to the Credit River, from its current 6 lanes to a 12-lane core/collector system.

The purpose of this letter is to advise you of the upcoming Public Information Centre (PIC) scheduled for **Wednesday November 13, 2013** from **4:00 p.m. to 8:00 p.m.** at the **Meadowvale Village Hall (6970 Second Line West, Mississauga, ON)**, and to invite you to attend. Additional information is enclosed and available on the project website at www.401expansion-mississauga.ca.

The PIC will be a drop-style open house format, at which the Recommended Plan will be presented.

Staff from the Project Team will be in attendance at the PIC to answer questions. You are encouraged to attend the PIC and provide your views and comments so they can be considered.

Sincerely,
AECOM Canada Ltd.

Brian Ruck, P.Eng.
Consultant Project Manager
Brian.Ruck@aecom.com

CL:mo

Encl. Notice of Public Information Centre

cc: Miao Zhou – MTO Project Engineer
Larry Sarris – MTO Environmental Planner
Mirjana Osojnicky – AECOM Environmental Planner

Ministry of Transportation

Planning and Environmental Office
Central Region
4th Floor, Bldg. 'D'
1201 Wilson Avenue
Downsview, ON M3M 1J8
Tel.: 416-235-4277
Fax.: 416-235-3576

Ministère des Transports

Bureau de la planification et de
l'environnement
Région du Centre
4^e étage, édifice D
1201, avenue Wilson
Downsview, ON M3M 1J8
Tél.: 416-235-4277
Télééc.: 416-235-3576



Sample Letter to First Nations

<<date>>

Contact Name
Contact Title
Company Name
Company Address

Dear Contact Name:

**Regarding: Notice of Public Information Centre (PIC)
Highway 401 Widening – Highway 403/410 Interchange to the Credit River
Detail Design Group Work Projects (G.W.P. 2150-01-00 and G.W.P. 2152-01-00)**

AECOM Canada Ltd. (AECOM) has been retained by the Ontario Ministry of Transportation (MTO) to undertake the Detail Design for the ultimate widening of Highway 401 from the Highway 403/410 interchange to the Credit River in the City of Mississauga, Regional Municipality of Peel, a distance of 7 km. The project will complete the expansion of Highway 401 from Highway 410/403 interchange to the Credit River, from its current 6 lanes to a 12-lane core/collector system.

The purpose of this letter is to advise you of the upcoming Public Information Centre (PIC) scheduled for **Wednesday November 13, 2013**, and to invite you to attend the session held from **3:00 p.m. to 4:00 p.m.** at the **Meadowvale Village Hall (6970 Second Line West, Mississauga, ON)** for municipal councillors/staff, First Nations representatives and external agency staff only. Additional information is enclosed and available on the project website at www.401expansion-mississauga.ca.

The environmental investigations for this assignment include:

- Fish and Fish Habitat Existing Conditions and Impact Assessment Report.
- Terrestrial Ecosystems Report, including ecological land classification (ELC), botanical inventory, migratory bird activity and wildlife activity and habitat.
- Groundwater Assessment. This project involves the identification of groundwater resources.
- A Stage 1/2 Archaeological Assessment following Ministry of Tourism and Culture guidelines for the completion of archaeological assessments.
- A Built Heritage and Cultural Heritage Landscape Assessment within the Project Limits.

The PIC will be a drop-in style open house format, at which the Recommended Plan will be presented. Staff from MTO and their consultant, AECOM, will be in attendance at the PIC to answer questions. You are encouraged to attend the PIC and provide your views and comments so they can be considered.

If you have any questions about the project, please contact one of the individuals named in the enclosed Notice. Should you prefer to meet with Ministry staff to discuss the study, please contact the undersigned at 416-235-4277, or by email at: Miao.Zhou@ontario.ca.

Sincerely,

Miao Zhou, M. Eng., P. Eng.
MTO Project Engineer
Ministry of Transportation, Central Region

Encl. Notice of Public Information Centre

cc: Brian Ruck – AECOM Project Manager
Leslie Currie – MTO Regional Archaeologist
Larry Sarris – MTO Environmental Planner
Mirjana Osojnicky – AECOM Environmental Planner

Ministry of Transportation

Planning and Environmental Office
Central Region
4th Floor, Bldg. 'D'
1201 Wilson Avenue
Downsview, ON M3M 1J8
Tel.: 416-235-4277
Fax.: 416-235-3576

Ministère des Transports

Bureau de la planification et de
l'environnement
Région du Centre
4^e étage, édifice D
1201, avenue Wilson
Downsview, ON M3M 1J8
Tél.: 416-235-4277
Télééc.: 416-235-3576



Sample Letter to Member of
Provincial Parliament (MPP)

DATE

Contact Name
Contact Title
Company Name
Company Address

Dear Contact Name:

**Regarding: Notice of Public Information Centre (PIC)
Highway 401 Widening – Highway 403/410 Interchange to the Credit River
Detail Design Group Work Projects (G.W.P. 2150-01-00 and G.W.P. 2152-01-00)**

AECOM Canada Ltd. (AECOM) has been retained by the Ontario Ministry of Transportation (MTO) to undertake the Detail Design for the ultimate widening of Highway 401 from the Highway 403/410 interchange to the Credit River in the City of Mississauga, Regional Municipality of Peel, a distance of 7 km. The project will complete the expansion of Highway 401 from Highway 410/403 interchange to the Credit River, from its current 6 lanes to a 12-lane core/collector system.

The purpose of this letter is to advise you of the upcoming Public Information Centre (PIC) scheduled for **Wednesday November 13, 2013**, and to invite you to attend the session held from **3:00 p.m. to 4:00 p.m.** at the **Meadowvale Village Hall (6970 Second Line West, Mississauga, ON)** for municipal councillors/staff, First Nations representatives and external agency staff only. Additional information is enclosed and available on the project website at www.401expansion-mississauga.ca.

The PIC will be a drop-in style open house format, at which the Recommended Plan will be presented. Staff from Project Team will be in attendance at the PIC to answer questions. You are encouraged to attend the PIC and provide your views and comments so they can be considered.

If you require additional information or wish to discuss the project, please contact either one of the following Project Team members listed in the enclosed Notice.

Sincerely,

Miao Zhou, M.Eng. P.Eng.
Project Engineer
Ontario Ministry of Transportation

CL:mo

Encl. Notice of Public Information Centre

cc: Brian Ruck - AECOM Project Manager
Larry Sarris – MTO Environmental Planner
Mirjana Osojnicky – AECOM Environmental Planner

Highway 401 Widening
From Highway 403/410 Interchange to the Credit River

Welcome to the

PUBLIC INFORMATION CENTRE

for the
Highway 401 Widening
From Highway 403/410 Interchange to the Credit River
Detail Design Project
G.W.P. 2150-01-00 and G.W.P. 2152-01-00

Wednesday November 13, 2013
4:00 p.m. to 8:00 p.m.

PLEASE SIGN IN

AECOM



Highway 401 Widening
From Highway 403/410 Interchange to the Credit River

Purpose of the Public Information Centre

The purpose of this Public Information Centre (PIC) is to seek input on the Detail Design for the recommended plan and answer questions about the project.

Purpose of the Project

- The purpose of the project is to carry out a Detail Design and Class Environmental Assessment (EA) for the ultimate widening of Highway 401 from the Highway 403/410 interchange to the Credit River, a distance of 7 km.
- The project will complete the expansion of Highway 401 within the project limits from its current 6-lanes to a 12-lane core/collector system, as documented in the Transportation Environmental Study Report (TESR) for the *Preliminary Design of Highway 401 From Highway 410/403 Interchange to East of the Credit River*, August 2005.
- Upon completion, the project will be documented in Design and Construction Reports (DCRs) and will include mitigation measures and future commitments to address the identified environmental impacts.



AECOM



Project Background

- In 1982, the Ministry of Transportation completed a Preliminary Design study for Highway 401 from Renforth Drive to 2nd Line West (G.W.P. 127-66-64). This study identified the need for significant capacity improvements to accommodate growing travel demands, including the need for an express/collector system west of the Highway 410/403.
- In August 2005, the Ministry of Transportation completed a Preliminary Design study for improvements to Highway 401 from the Highway 410/403 interchange westerly to east of the Credit River (G.W.P. 2149-01-00 and 2150-01-00). This study was undertaken to review and update previous work and look for opportunities to integrate High Occupancy Vehicle (HOV) facilities within the corridor.
 - The study included two (2) PICs held in June 2003 and June 2004.
 - The Transportation Environmental Study Report (TESR) was approved with Conditions by the Minister of the Environment in 2007 following receipt of two (2) bump-up requests (Part II Orders).
- In June 2011, the Ministry of Transportation initiated the current Detail Design project (G.W.P. 2150-01-00).

Design Features

This project involves:

Under G.W.P. 2150-01-00

- Expansion of Highway 401 from its current 6-lanes to a 12-lane core/collector system including:
 - Collector lanes consisting of three (3) general purpose lanes in each direction;
 - Core lanes consisting of two (2) general purpose lanes and one (1) High Occupancy Vehicle (HOV) lane in each direction;
 - HOV lanes from west of Mavis Road to east of Highway 410 to be opened following construction completion;
 - Removal of the 2nd Line West structure over Highway 401 to accommodate the highway widening;
 - New ramps from Highway 401 eastbound to Highway 403 southbound and from Highway 403 northbound to Highway 401 westbound at the 401/410/403 interchange;
 - New structures carrying the proposed collector lanes over Fletcher's Creek;
 - Stormwater management strategy;
 - High mast illumination on Highway 401 throughout the project limits; and
 - Reconstruction of the existing highway pavement.

Under G.W.P. 2152-01-00

- Extension of the Mavis Road bridge as an advanced construction contract to accommodate the proposed additional collector lanes.

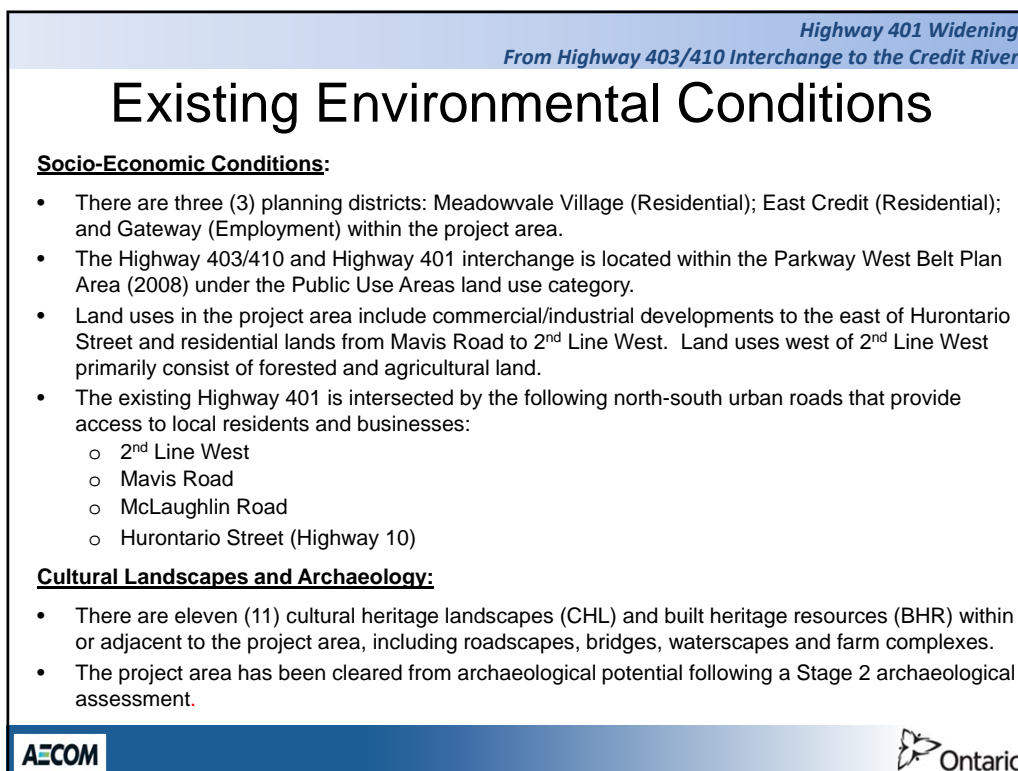
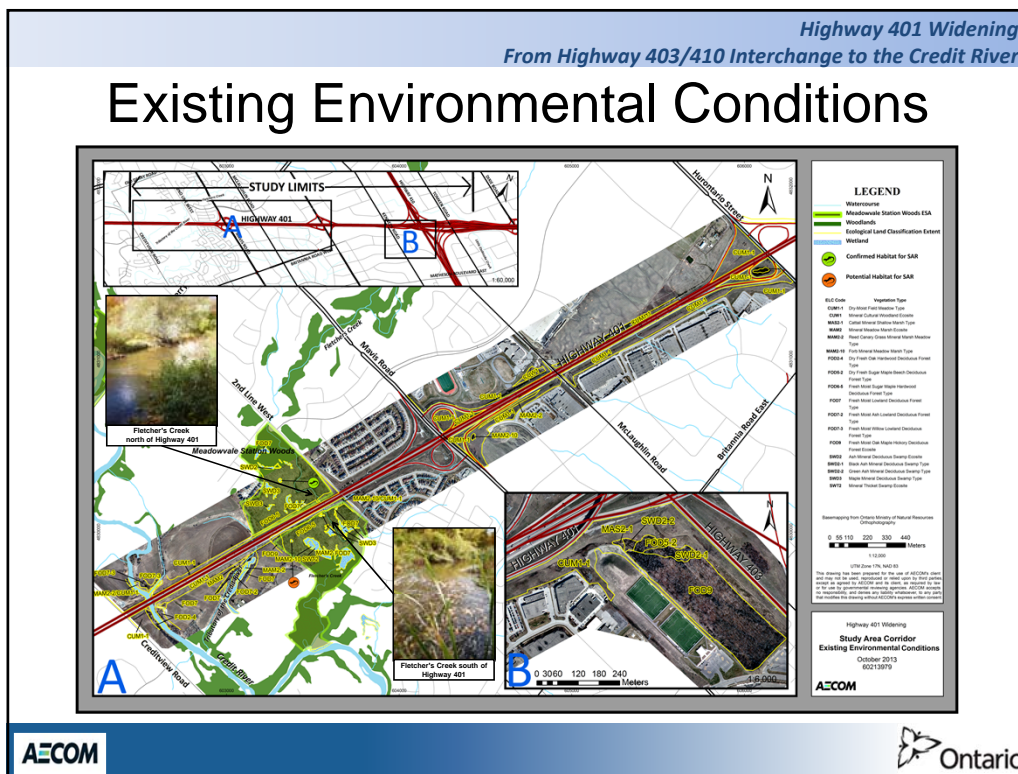
EA Process

- This project is following the approved environmental planning process for **Group 'B' projects**, under the Ministry's Class Environmental Assessment (EA) for Provincial Transportation Facilities (2000), with the opportunity for public input throughout.
- **Group 'B' Projects** generally include major improvements to existing provincial transportation facilities. They do not require formal EA Act review and approval, provided the Class EA process is followed.
- There is an opportunity at any time during the Class EA Process for interested persons to provide comments and review outstanding issues.
- The project will proceed with documentation as follows:
 - Extension of the Mavis Road bridge will be documented in a **Design and Construction Report (DCR)** for construction start in 2014;
 - The remainder of the highway expansion work will be documented in a separate **DCR**; and
 - An **Addendum to the August 2005 TESR** will be prepared to document the proposed extension of Belgrave Road to connect to Mavis Road. Only the changes documented in the Addendum are eligible for a bump-up (request for a Part II Order by the Minister of the Environment).
- There is no bump up opportunity (request for Part II Order by the Minister of the Environment) for a DCR.
- Details regarding the release of these documents will be provided in future notices.

Existing Environmental Conditions

Natural Environment:

- The project area contains two watercourses, including:
 - **Tributary of the Credit River** – crosses Highway 401 approximately 0.7 km east of the Credit River. It originates on the north side of the Highway to the west of Meadowvale Station Woods and discharges approximately 700 m downstream of the highway. This tributary is an intermittent stream with a coolwater thermal classification and is located within the Credit River Watershed.
 - **Fletcher's Creek** – large meandering watercourse that flows through Meadowvale Station Woods and discharges to the Credit River approximately 1.5 km downstream of the highway. Fletcher's Creek is a permanent stream with coolwater thermal classification and is located within the Credit River Watershed.
- There is evidence of wildlife crossings through existing culverts at the Tributary of the Credit River and Fletcher's Creek.
- Species at Risk and/or habitat are present within the project area including the Jefferson Salamander (*Ambystoma jeffersonianum*) and Redside Dace (*Clinostomus elongatus*), as designated under the *Endangered Species Act* (2007).
- The Environmentally Sensitive Area (ESA) and Life Science Area of Natural and Scientific Interest (ANSI) named *Meadowvale Station Woods*, bisects Highway 401.
- There are no Provincially Significant Wetlands within the project limits, however there is an unevaluated wetland near the Highway 403/410 interchange.
- MOE Well Water Records indicate that there are fourteen (14) wells located in the project area, the majority are inactive, decommissioned, or demolished.



Noise Assessment

- A Noise Sensitive Area (NSA) is defined as a noise sensitive land use with an outdoor living area, including: single family houses (typically backyard); townhouses (typically backyard); multiple unit buildings, such as apartments with outdoor living areas for use by all occupants; hospitals and nursing homes, where there are outdoor living areas for patients.
- Highway noise levels take into consideration the vehicle type, road grade, distance from an NSA, type of ground between the road and the NSA and travel speeds.
- A noise analysis was carried out to assess the potential impacts of the proposed highway conditions. The analysis determined that no additional noise mitigation measures are warranted.

New Highway 401 West to Highway 403 South Ramp and Highway 403 South to Highway 401 West Ramp



See Roll Plan for Greater Detail

Key Changes from the 2005 TESR

Key changes include:

- An eastbound transfer from collector to core has been added;
- To address concerns with respect to wildlife crossings, the Fletcher's Creek crossing was enlarged and wildlife fencing will direct animals to safe crossings at the Credit River and Fletcher's Creek;
- A Stormwater (SWM) management pond identified for the north side of Highway 401 at 2nd Line West has been removed from the design. The SWM strategy for the area draining to Fletcher's Creek will include a series of small wetlands within the Mavis Road interchange. This will allow for additional vegetation restoration once 2nd Line West is closed;
- MTO's Green Pavement Design Rating System 'GreenPave' was considered in the development and evaluation of pavement recommendations; and
- 'Green Road' design concepts were incorporated to mitigate environmental impacts (A copy of the Report is on the Reference Table).

Fletcher's Creek Crossing

- The August 2005 TESR recommended retaining the existing twin cell culvert and constructing a third cell under the future core lanes to address hydraulic deficiencies. In addition, two (2) rigid frame structures were proposed to carry the future Highway 401 collector lanes over Fletcher's Creek.
- The project team has re-evaluated the crossing and determined that full replacement with two bridges is preferred:
 - Provides enhancement to the SAR designated watercourse;
 - Provides ultimate infrastructure upgrade within the Highway 401 core/collector expansion;
 - Culvert inspection revealed that the existing structural culverts are weak and would require rehabilitation;
 - Allows for additional room for construction access;
 - Will significantly reduce upstream flooding conditions;
 - Allows opportunity for increased light, air penetration and a larger opening to make it more attractive to wildlife crossing the highway; and
 - Reduces future maintenance and disruption of Highway 401 traffic.
- MTO has obtained MNR's agreement in principle on the design concept including the span of the new bridges.



Existing twin cell culvert at Fletcher's Creek



Proposed replacement with two full span bridges

Conceptual Fletcher's Creek Crossing



Conceptual bird's eye view of 2 clear span bridges



Conceptual view from north side of Highway 401

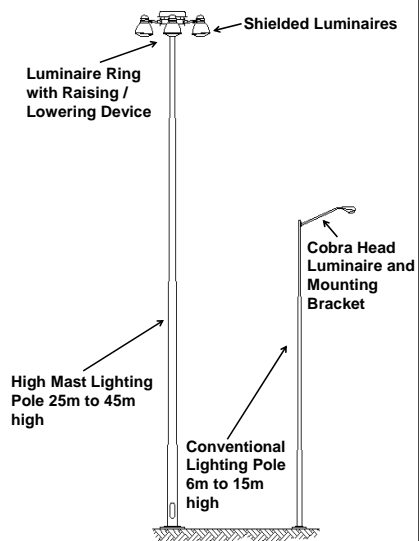


Conceptual view from south side of Highway 401



Lighting System Improvements



- High mast system will be implemented within the entire project corridor and will extend along Highway 401 from Mavis Road to the Credit River.
- High mast lighting at the Mavis Road interchange will be upgraded to meet current standards and to reduce the amount of light trespass onto private properties.
- Special measures will be applied to control light and minimize light spillage into environmentally sensitive and residential areas:
 - Environmentally Sensitive Area extends from 2nd Line West to the Credit River; and
 - Residential zone covers the section of Highway 401 between Mavis Road and 2nd Line West.
- In some sections, high mast lighting will be supplemented by lower conventional lighting poles to minimize light spillage.
- High mast lighting poles will be located in the median for the main sections of the Highway 401 core/collector system and located strategically within the Mavis Road interchange to provide uniform coverage of Highway 401 and access ramps.
- Shielded high mast lighting fixtures have been selected to significantly reduce glare and create a well defined lighting corridor while controlling lighting levels beyond the MTO Right-of-Way.



*Highway 401 Widening
From Highway 403/410 Interchange to the Credit River*

Existing and Proposed Illumination Plan

See Roll Plan for Greater Detail



 

*Highway 401 Widening
From Highway 403/410 Interchange to the Credit River*

Construction Staging – Mavis Road

- Extension of the Mavis Road bridge will be required in order to accommodate widening of the highway.
- It is expected that construction will take place over a 2-year period.
- During construction of the interchange, two lanes of traffic will be maintained in both directions at all times.

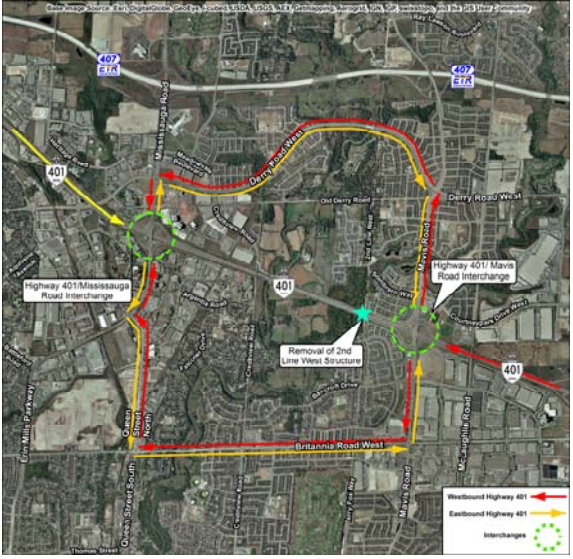
See Traffic Staging Drawings for More Detail


 

Highway 401 Widening
From Highway 403/410 Interchange to the Credit River

Proposed Construction Staging – Removal of 2nd Line West

- Removal of the 2nd Line West structure will require the temporary full closure of Highway 401 in the eastbound and westbound direction from Mavis Road to Mississauga Road.
- Closure will take place over the weekend for approximately a 12-18 hour period; typically starting 11pm on Saturday to 5pm on Sunday.
- A Communication Plan will be developed to include:
 - PVMS signs advising motorists in advance of and during construction;
 - Notification to transit and emergency services;
 - Media notification; and
 - Signage along all detour routes.
- Detours will ensure that thru traffic avoids travel through residential neighbourhoods.
- Details are subject to approval by the City of Mississauga and the Regional Municipality of Peel.
- Detour routes include Mavis Road, Derry Road West, Britannia Road West, Queen Street North and Mississauga Road.




AECOM 

Highway 401 Widening
From Highway 403/410 Interchange to the Credit River

Potential Environmental Impacts and Mitigation

Potential Issues/Effects	Mitigation
Natural Environment	
Soil Erosion and Sediment Control <ul style="list-style-type: none"> Excavation and grading activities associated with construction may result in erosion and generation of sediment carried into the watercourses within the project limits 	<ul style="list-style-type: none"> Sedimentation control measures consisting of the installation of silt fencing, as per Ontario Standard Drawings, will be installed prior to construction and any surface grading. Plans will be developed and reviewed by the CVC and MNR. Erosion and sediment control practices will focus on two separate targets: minimizing site erosion; and, keeping any eroded material on site. Best management practices for erosion and sediment control include: <ul style="list-style-type: none"> Silt fence barriers, erosion control blanket, and rock flow checks will be implemented during construction to prevent migration of sediment to the watercourses within the project area and all other natural features; Rip rap or other stabilizing systems will be installed at outlets or spillways; and Stabilization and re-vegetation of all disturbed surfaces will be established as soon as possible with the most appropriate treatments available.
Watercourses and Fisheries <ul style="list-style-type: none"> Potential for impacts to fish habitat in Tributary to the Credit River and Fletcher's Creek 	<ul style="list-style-type: none"> Maintain fish habitat through structures with fisheries resources by providing natural substrate and channel form. Apply timing constraints for construction. Contract package to include appropriate timing restrictions and mitigation design (i.e. open bottom culvert, clear span bridge, where possible). Manage fisheries impacts and mitigation in accordance with the Federal <i>Fisheries Act</i> and the MTO/DFO/MNR Fisheries Protocol. Reinstate and expand culvert inlet and outlet at Fletcher's Creek and enhance pools at inlet and outlet of culvert at tributary of the Credit River.
Wildlife <ul style="list-style-type: none"> Potential for wildlife passage through existing culverts at Tributary to the Credit River and Fletcher's Creek 	<ul style="list-style-type: none"> A wildlife crossing will be established at Fletcher's Creek that allows for increased light, air penetration and a larger opening to make it more attractive to wildlife crossing the highway and provides enhancement to the SAR designated watercourse. Fencing will be provided along Highway 401 within the Meadowvale Station Woods to encourage/direct wildlife to use the crossing.
Vegetation <ul style="list-style-type: none"> Loss of vegetation due to construction 	<ul style="list-style-type: none"> Develop an overall-benefit plan and contract documents to include operational constraints and special provisions with respect to vegetation removal.
Species At Risk <ul style="list-style-type: none"> Loss of habitat for species at risk due to construction 	<ul style="list-style-type: none"> A permit will be obtained under the <i>Endangered Species Act</i> for the area of the Meadowvale Station Woods and crossing of Fletcher's Creek prior to construction for two species at risk. Tree and site clearing will take place between September and March 31st and avoid the months of April until August, in accordance with the <i>Migratory Birds Convention Act</i>.
Groundwater/Surface Water <ul style="list-style-type: none"> Potential for increased pollutants to enter receiving watercourses and groundwater recharge areas 	<ul style="list-style-type: none"> Drainage will be designed to manage stormwater quality and quantity, and will include a series of small wetlands within the Mavis Road interchange for the area draining to Fletcher's Creek. Obtain Permit to Take Water for dewatering activities exceeding 50,000L/day. Re-fuelling during construction will occur in designated areas.
Management of Excess Material and Property Contamination	<ul style="list-style-type: none"> Management of excess materials will be dealt with in accordance with normal MTO practices (regulated by OPSS 180). Lead paint on guard rails will be managed in accordance with the <i>Ontario Occupational Health and Safety Act</i> (Reg. 843) and <i>Ontario Environmental Protection Act</i> (Reg. 347).

AECOM 

*Highway 401 Widening
From Highway 403/410 Interchange to the Credit River*


Potential Environmental Impacts and Mitigation

Potential Issues/Effects	Mitigation
Socio-Economic Environment	
Noise <ul style="list-style-type: none"> Highway operational noise Potential noise increase during construction activities 	<ul style="list-style-type: none"> A noise analysis was carried out to assess the potential impacts of the proposed highway conditions. The analysis determined that no additional noise mitigation measures are warranted. The Contractor will be required to maintain equipment in good operating condition to prevent unnecessary noise and restrict idling of equipment to the minimum necessary to perform the work. Complaints about noise will be investigated in accordance with the MTO/MOE Noise Protocol. Adherence to the municipal noise by-law.
Archaeology <ul style="list-style-type: none"> Potential impact to archaeological resources 	<ul style="list-style-type: none"> A Stage 2 Archaeological Assessment recommended that no further archaeological concern is warranted for lands with the project area. If any archaeological and/or historical resources be discovered during construction, they may be a new archaeological site and therefore subject to Section 48(1) of the <i>Ontario Heritage Act</i>. Construction must cease immediately and a licensed consultant archaeologist must be engaged to carry out archaeological fieldwork in compliance with Section 48(1) of the <i>Ontario Heritage Act</i>. Work in the area would not resume until cleared by the Ministry of Tourism, Culture and Sport.
Property Impacts <ul style="list-style-type: none"> Limited property requirements for widening 	<ul style="list-style-type: none"> MTO will negotiate with individual owners for property purchase in accordance with standard MTO procedures.
Emergency Access <ul style="list-style-type: none"> Temporary or permanent change to access 	<ul style="list-style-type: none"> Notify OPP and emergency medical services of construction staging, start of construction, temporary closure of Highway 401 for the removal of the 2nd Line West structure, etc. to minimize delay in emergency response times during and after construction.
Air Quality (Dust)	<ul style="list-style-type: none"> Dust suppressants will be used during construction. Construction equipment will be required to conform to emissions standards and the idling of equipment will be restricted to ensure that air quality is not unduly impacted.
Utilities <ul style="list-style-type: none"> Potential impacts to existing utilities 	<ul style="list-style-type: none"> Utility relocations will be coordinated.
Traffic <ul style="list-style-type: none"> Motorists may experience delays and disruption during construction 	<ul style="list-style-type: none"> Collector lanes will be generally constructed first thereby minimizing traffic disruptions on Highway 401. Closure of lanes only once additional capacity of the network has been provided. Short-term closures will be limited to off-peak and/or nighttime hours.


AECOM

*Highway 401 Widening
From Highway 403/410 Interchange to the Credit River*


Proposed Belgrave Road Extension

- At the request of the City of Mississauga, the Ministry of Transportation has investigated the need for a future extension of Belgrave Road from the Highway 401 EB off ramp at Mavis Road easterly to the existing cul-de-sac west of Suffolk Court – a distance of approximately 200m.
- Three (3) alternatives were considered. They include:
 - 

Alternative A – Mavis Road Underpass to Belgrave Road

 - The full grade separated option would provide unimpeded unidirectional traffic flow onto Belgrave Road from the W-N/S ramp via a tunnel under the existing Mavis Road, similar to that currently under construction at Hurontario Street.
 - Access from Highway 401 east would be split into two ramps when approaching Mavis Road. Ramp W-N/S would approach Mavis Road at-grade while a second Ramp W-E would tunnel under Mavis Road [and S-E Ramp] to connect to Belgrave Road.
 - 

Alternative B – At-Grade Intersection with Mavis Road Structure Over Realigned Ramp S-E

 - An at-grade connection would provide 2-way access to Belgrave Road from Mavis Road. Some turning movements would be restricted at the intersection (i.e. Belgrave WB Left) to optimize traffic operations.
 - A south to east at-grade ramp would be constructed under the Belgrave Road extension.
 - MTO would construct the S-E ramp on a lower profile and the City would build a bridge over the ramp to connect Belgrave Road at a later date.
 - 

Alternative C – At Grade Full Intersection at Mavis Road Ramp S-E Relocated North of the Existing Intersection

 - MTO Central Region has utilized this type of connection within Parclo-A type interchanges in some recent highway projects to maintain connections to existing connected roadways.
 - Construction of a tighter Ramp S-E avoids the construction of a bridge structure.

AECOM

Proposed Belgrave Road Extension



- **Alternative B** is selected to be carried forward as the Recommended Plan. This alternative provides operational benefits including providing free flow of traffic and provision of a 'right out' access to northbound Mavis Road; allows for the protection of a future Belgrave extension within the current MTO contract; and, has minimal impact to existing environmental features.
- For this proposed design enhancement, MTO will prepare an Addendum to the August 2005 TESR, on behalf of the City of Mississauga. The TESR Addendum will document the proposed change to the 2005 TESR and consultation which has taken place. It will be available for a 30-day public review period. Only the changes identified in this presentation and documented in the Addendum will be eligible for a 'bump up' (request for a Part II Order by the Minister of the Environment).
- A Notice of TESR Addendum Filing will be posted in the **Mississauga News**, **L'Express de Toronto**, as well as the project website at www.401expansion-mississauga.ca.
- Timing for the construction of the Belgrave Road extension will be determined at a later date.

Copy of the Geometric Feasibility Design Report (August 2012) on the Reference Table

Next Steps

Following tonight's information session we will:

- Review and consider your comments and input;
- Respond to your written questions;
- Produce the following EA documentation:
 - DCR to document the reconstruction of the Mavis Road interchange;
 - DCR to document the remainder of the highway expansion work, with the exception of the proposed Belgrave Road extension; and
 - TESR Addendum to the 2005 TESR to document the proposed extension of Belgrave Road.



Details regarding the release of these documents and will be provided in future notices, as well as the project website at www.401expansion-mississauga.ca.

Once the Detail Design and contract package for the Mavis Road interchange and the remainder of the highway expansion is finalized, the project will proceed to construction.

The City of Mississauga is considering a separate study for pedestrian/cycling options across Highway 401 at the location of the removed 2nd Line West structure. This study will be undertaken in accordance with the *Municipal Class EA* process.

Highway 401 Widening
From Highway 403/410 Interchange to the Credit River

Remain Involved in the Project

Thank you for attending this PIC and participating in the Detail Design process. We encourage you to fill out the comment sheet provided and drop it off in the comment box. Alternatively, you can mail, fax, or email your comments by December 13, 2013 to either of the individuals listed below:

Miao Zhou, P.Eng.
Project Engineer
Ontario Ministry of Transportation
Central Region
Building 'D', 4th Floor, 1201 Wilson Ave
Downsview, ON M3M 1J8
Phone: (416) 235-4277
Fax: (416) 235-3576
Email: Miao.Zhou@ontario.ca

Brian Ruck, P. Eng. C.V.S.,
Consultant Project Manager
AECOM Canada Ltd.
300 Water Street
Whitby, ON L1N 9J2
Toll Free: 1-800-668-1983 or
Phone: (905) 668-4021 ext. 2250
Fax: (905) 665-4867
E-mail: Brian.Ruck@aecom.com

All comments received will be reviewed and considered in the Detail Design process to implement the proposed improvements within the Highway 401 Project Limits. Public comments will be collected in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.



Select Correspondence



REPLY FORM

PLEASE REPLY BY July 25, 2011

To: MIRJANA OSOJNICKI, AECOM

Date: July 11, 2011

Fax: (905) 238-0038

E-mail: mirjana.osojnicki@aecom.com

Re: Notice of Study Commencement Highway 401 Widening, from Highway 403/410 Interchange to the Credit River, City of Mississauga, Regional Municipality of Peel, 7 km, GWP 2150-01-00

NAME: Adrian Smith

Use my contact information as the key project contact for future correspondence

TITLE: Director, Planning Policy and Growth Management

MUNICIPALITY/AGENCY: City of Brampton

ADDRESS: 2 Wellington Street West, Brampton, ON

POSTAL CODE: L6Y 4R2

PHONE: 905-874-2052

FAX: 905-874-2099

E-MAIL: adrian.smith@brampton.on

Please indicate the appropriate response:

My group/agency **is interested** in providing input regarding this Study. Please leave me on the Study Mailing List.

My group/agency **is not interested** in providing input regarding this Study but would like to be kept informed. Please leave me on the Study Mailing List.

Please **remove** my group/agency from Study Mailing List.

Area of interest or concern/preliminary comments:



REPLY FORM

PLEASE REPLY BY July 25, 2011

To: MIRJANA OSOJNICKI, AECOM

Date: July 19/2011

Fax: (905) 238-0038

E-mail: mirjana.osojnicki@aecom.com

Re: Notice of Study Commencement Highway 401 Widening, from Highway 403/410 Interchange to the Credit River, City of Mississauga, Regional Municipality of Peel, 7 km, GWP 2150-01-00

NAME: Liam Marray

Use my contact information as the key project contact for future correspondence

TITLE: Manager, Planning Ecology

MUNICIPALITY/AGENCY: Credit Valley Conservation

ADDRESS: 1255 Old Derry Rd

POSTAL CODE: L5N 6R4

PHONE: 905-670-1615 x239

FAX: 905-670-2210

E-MAIL: lmarray@creditvalleyca.ca

Please indicate the appropriate response:

My group/agency **is interested** in providing input regarding this Study. Please leave me on the Study Mailing List.

My group/agency **is not interested** in providing input regarding this Study but would like to be kept informed. Please leave me on the Study Mailing List.

Please **remove** my group/agency from Study Mailing List.

Area of interest or concern/preliminary comments:

Detailed comments forthcoming via email



REPLY FORM

PLEASE REPLY BY July 25, 2011

To: MIRJANA OSOJNICKI, AECOM

Date: July 5/2011

Fax: (905) 238-0038

E-mail: mirjana.osojnicki@aecom.com

Re: Notice of Study Commencement Highway 401 Widening, from Highway 403/410 Interchange to the Credit River, City of Mississauga, Regional Municipality of Peel, 7 km, GWP 2150-01-00

NAME: HYDRO ONE NETWORKS

Use my contact information as the key project contact for future correspondence

TITLE: SCHEDULING

MUNICIPALITY/AGENCY: HYDRO ONE

ADDRESS: 913 CRAWFORD DR

POSTAL CODE: K9J 3X1

PHONE: 1-888-871-3514 x 3307

FAX:

E-MAIL: ZONE 3A SCHEDULING @ HYDROONE.COM

Please indicate the appropriate response:

- My group/agency **is interested** in providing input regarding this Study. Please leave me on the Study Mailing List.
- My group/agency **is not interested** in providing input regarding this Study but would like to be kept informed. Please leave me on the Study Mailing List.
- Please **remove** my group/agency from Study Mailing List.

Area of interest or concern/preliminary comments:

Osojnicki, Mirjana

From: Zirger, Rosi (MTC) [Rosi.Zirger@ontario.ca]
Sent: August 3, 2011 4:04 PM
To: Osojnicki, Mirjana
Subject: Highway 401 widening - Highway 403/410 Interchange to Credit River (MTO) - Detail Design (GWP 2150-01-00)

Follow Up Flag: Follow up
Flag Status: Flagged

Project: Highway 401 widening – Highway 403/410 Interchange to Credit River (MTO) - Detail Design (GWP 2150-01-00)

Location: City of Mississauga

MTC File: 21EA063

The Ministry of Tourism and Culture (MTC) received a Notice of Study Commencement for the project mentioned above. As part of the Class Environmental Assessment process, the MTC has an interest in the conservation of cultural heritage resources including:

- archaeological resources,
- built heritage resources, and
- cultural heritage landscapes.

MTC would be interested in remaining on the circulation list and being informed of the project as it proceeds through the EA process. We ask that you update your contact list to remove the names of Karla Barboza and Mairflor Toneatto, and send future notices to **Rosi Zirger A/Heritage Planner at the address below**.

Best regards,

[Rosi Zirger](#)
[A/Heritage Planner | Central and Southeast](#)
[Ministry of Tourism and Culture | Culture Services Unit](#)
[Tel. 416.314.7159 | Fax 416.314.7175](#)
rosi.zirger@ontario.ca

[Ministry of Tourism and Culture](#)
Programs and Services Branch
Culture Services Unit
401 Bay Street, 17th Floor
Toronto, Ontario M7A 0A7

REPLY FORM

PLEASE REPLY BY July 25, 2011

To: MIRJANA OSOJNICKI, AECOM

Date: July 8/2011

Fax: (905) 238-0038

E-mail: mirjana.osojnicki@aecom.com

Re: Notice of Study Commencement Highway 401 Widening, from Highway 403/410 Interchange to the Credit River, City of Mississauga, Regional Municipality of Peel, 7 km, GWP 2150-01-00

NAME: John M^cDougall

Use my contact information as the key project contact for future correspondence

TITLE: Fire chief

MUNICIPALITY/AGENCY: Mississauga Fire & Emergency Services

ADDRESS: 15 Fairview Rd W

POSTAL CODE: L5B 1K7

PHONE: (905) 615-3750

FAX: (905) 615-3773

E-MAIL: john.m^cDougall@mississauga.ca

Please indicate the appropriate response:

- My group/agency **is interested** in providing input regarding this Study. Please leave me on the Study Mailing List.
- My group/agency **is not interested** in providing input regarding this Study but would like to be kept informed. Please leave me on the Study Mailing List.
- Please **remove** my group/agency from Study Mailing List.

Area of interest or concern/preliminary comments:

We would like to be informed of any changes throughout the process that may impact access for emergency service vehicles.



REPLY FORM

PLEASE REPLY BY July 25, 2011

To: MIRJANA OSOJNICKI, AECOM

Date: July 4, 2011

Fax: (905) 238-0038

E-mail: mirjana.osojnicki@aecom.com

Re: Notice of Study Commencement Highway 401 Widening, from Highway 403/410 Interchange to the Credit River, City of Mississauga, Regional Municipality of Peel, 7 km, GWP 2150-01-00

NAME: Jackie Van de Valk - know you dont return till July 11/11. (SD)

Use my contact information as the key project contact for future correspondence

TITLE: Rural Planner

MUNICIPALITY/AGENCY: OMAFRA

ADDRESS: 6481 Wellington Rd 7, Unit 10 Elora, ON

POSTAL CODE: N0B 1S0

PHONE: 519-846-3415

FAX: _____

E-MAIL: _____

Please indicate the appropriate response:

- My group/agency **is interested** in providing input regarding this Study. Please leave me on the Study Mailing List.
- My group/agency **is not interested** in providing input regarding this Study but would like to be kept informed. Please leave me on the Study Mailing List.
- Please **remove** my group/agency from Study Mailing List.

Area of interest or concern/preliminary comments:

June 23, 2011

Mr. Brian Buck, P.Eng C.V.S.
Consultant Project Manager
AECOM Canada Ltd.
300 Water Street
Whitby, ON L1N 9J2

⇒ Mirjana Osojnicki
(Mississauga)

Dear Mr. Buck:

**RE: Highway 401 Widening – From Hwy 403/410 Interchange to the Credit River
Notice of Study Commencement
Ontario Government**

Thank you for your letter of June 21, 2011 informing us of the commencement of the Design and Construction Report for the above noted project. The Peel District School Board is interested in this project as we have a school in the vicinity of the study area. Please keep us informed of the status of this project and provide us with any information available so that we may monitor its progress and provide comments as necessary.

If you require any further information please contact me at 905-890-1010, ext. 2217.

Yours truly,



Paul Mountford, MCIP RPP
Intermediate Planning Officer
Planning and Accommodation Department

c. S. Hare, Peel District School Board
M. Zhou, Ontario Ministry of Transportation

Hwy 401 CR.doc

Trustees

Janet McDougald, Chair
Suzanne Nurse, Vice-Chair
Stan Cameron
Beryl Ford
David Green
Meredith Johnson

Steve Kavanagh
Sue Lawton
Brad MacDonald
Harinder Malhi
Jeff White
Rick Williams

Director of Education and Secretary

Tony Pontes

**Associate Director,
Instructional Support Services**
Pam Tomasevic

**Associate Director,
Operational Support Services**
Carla Kisko



REPLY FORM

PLEASE REPLY BY July 25, 2011

To: MIRJANA OSOJNICKI, AECOM

Date: July 20/2011

Fax: (905) 238-0038

E-mail: mirjana.osojnicki@aecom.com

Re: Notice of Study Commencement Highway 401 Widening, from Highway 403/410 Interchange to the Credit River, City of Mississauga, Regional Municipality of Peel, 7 km, GWP 2150-01-00

NAME: Sabbir Saiyed

Use my contact information as the key project contact for future correspondence

TITLE: Manager of Transportation System Planning

MUNICIPALITY/AGENCY: Region of Peel

ADDRESS: 10 Peel Centre Drive, Suite A, 6th floor

POSTAL CODE: L6T 4B9

PHONE: (905) 791-7800 Extension: 4352

FAX: (905) 791-7920

E-MAIL: sabbir.saiyed@peelregion.ca

Please indicate the appropriate response:

- My group/agency **is interested** in providing input regarding this Study. Please leave me on the Study Mailing List.
- My group/agency **is not interested** in providing input regarding this Study but would like to be kept informed. Please leave me on the Study Mailing List.
- Please **remove** my group/agency from Study Mailing List.

Area of interest or concern/preliminary comments:



REPLY FORM

PLEASE REPLY BY July 25, 2011

To: MIRJANA OSOJNICKI, AECOM

Date: July 13/11

Fax: (905) 238-0038

E-mail: mirjana.osojnicki@aecom.com

Re: Notice of Study Commencement Highway 401 Widening, from Highway 403/410 Interchange to the Credit River, City of Mississauga, Regional Municipality of Peel, 7 km, GWP 2150-01-00

NAME: Ken McBRIDE

Use my contact information as the key project contact for future correspondence

TITLE: _____

MUNICIPALITY/AGENCY: TRANS CANADA PIPELINES

ADDRESS: 11200 WESTON ROAD, MAPLE, ON

POSTAL CODE: L6A 1S7

PHONE: 905-832-7344

FAX: _____

E-MAIL: _____

Please indicate the appropriate response:

- My group/agency **is interested** in providing input regarding this Study. Please leave me on the Study Mailing List.
- My group/agency **is not interested** in providing input regarding this Study but would like to be kept informed. Please leave me on the Study Mailing List.
- Please **remove** my group/agency from Study Mailing List.

Area of interest or concern/preliminary comments:



ALDERVILLE FIRST NATION
P.O. Box 46
11696 Second Line
Roseneath, Ontario K0K 2X0

Chief:	James R. Marsden
Councillor:	Dave Mowat
Councillor:	Pam Crowe
Councillor:	Leonard Gray
Councillor:	Randall Smoke

July 28th, 2011

Att: Miao Zhou, M.Eng. P.Eng.

**Re: Notice of Study Commencement
Highway 401 Widening - Highway 403/410 Interchange to the Credit River
Group Work Project (G.W.P.) 2150-01-00**

Dear Miao,

Thank you for your consultation request to Alderville First Nation regarding the **Project for the Ultimate Widening of Highway 401 from the Highway 403/410 Interchange to the Credit River**, which is being proposed within our Traditional and Treaty Territory. We appreciate the fact that the **Ministry of Transportation**, recognizes the importance of First Nations Consultation and that your office is conforming to the requirements within the Duty to Consult Process.

As per the Alderville First Nation Consultation Protocol, your proposed project is deemed a level 3, having minimal potential to impact our First Nations' rights, therefore, please keep Alderville apprised of any archaeological findings, burial sites or any environmental impacts, should any occur.

Although we may not always have representation at all stakeholders meetings, it is our wish to be kept apprised throughout all phases of this project. I can be contacted at the mailing address above or electronically via email, at the email address below.

In good faith and respect,

Dave Simpson
Lands and Resources
Communications Officer
Alderville First Nation

dsimpson@aldervillefirstnation.ca

Tele: (905) 352-2662
Fax: (905) 352-3242

Miao



HIAWATHA FIRST NATION
123 Paudash Street
Hiawatha On. K0L2G0

Chief:	Sandra Moore
Councillor:	Jill Smith
Councillor:	Duane Cowie
Councillor:	Lorne Paudash
Councillor:	Donald Paudash

RECEIVED

AUG - 2 2011

MTO-CENTRAL REGION
PLANNING & DESIGN
ENGINEERING SECTION

Date: July 20, 2011:

Ontario Ministry of Transportation:

Miao Zhou:

Thank you for the information you sent to Hiawatha First Nation regarding "401 Widening Project 2150-01-00" which is being proposed within Hiawatha First Nations' Traditional and Treaty Territories. Hiawatha First Nation appreciates the fact that MTO recognizes the importance of First Nations Consultation and that your office is conforming to the requirements within the Duty to Consult Process however this correspondence is not considered meaningful consultation.

As per the Hiawatha First Nation Consultation Protocol, your proposed project is deemed, having minimal potential to impact Hiawatha First Nations' rights at this time, however, please keep us apprised of any updates, archaeological findings, and/ or of any environmental impacts, should any occur. We reserve the right to comment later, if something further along in the process is deemed to have a potential impact.

We would like to be contacted if artifacts are found and to be sent any archaeological reports as they are completed. We also have trained archaeological liaisons that we require being present at the archaeological sites during the assessments if first nation artifacts are found.

We can be contacted at the mailing address above or electronically via email, at the email address below.

In good faith and respect,

Lori Ritter and Diane Sheridan
Land Resource Workers
Hiawatha First Nation

hfnconsultation@nexicom.net
Tele: (705) 295 4421
Fax: (705) 295-7144



ALDERVILLE FIRST NATION

11696 Second Line

P.O. Box 46

Roseneath, Ontario K0K 2X0

Phone: (905) 352-2011

Fax: (905) 352-3242

Chief: James R. Marsden

Councillor: Julie Bothwell

Councillor: Jody Holmes

Councillor: Dave Mowat

Councillor: Angela Smoke

November 5, 2013

Ministry of Transportation
Planning and Environmental Office Central Region
4th Floor, Bldg. D
1201 Wilson Avenue
Downsview, ON M3M 1J8

Att: Miao Zhou, M. Eng., P. Eng.

**Re: Notice of Public Information Centre (PIC)
Highway 401 Widening – Highway 403/410 Interchange to the Credit River
Detail Design Group Work Projects (G.W.P. 2150-01-00 and G.W.P. 2152-01-00)**

Dear Miao Zhou,

Thank you for your consultation request to Alderville First Nation regarding the widening of Highway 401 from the Highway 403/410 interchange to the Credit River in the City of Mississauga which is being proposed within our Traditional and Treaty Territory. We appreciate the fact that the Ministry of Transportation recognizes the importance of First Nations Consultation and that your office is conforming to the requirements within the Duty to Consult Process.

As per the Alderville First Nation Consultation Protocol, your proposed project is deemed a level 3, having minimal potential to impact our First Nations' rights, therefore, please keep Alderville apprised of any archaeological findings, burial sites or any environmental impacts, should any occur. I can be contacted at the mailing address above or electronically via email, at the email address below.

In good faith and respect,

Dave Simpson
Lands and Resources
Communications Officer
Alderville First Nation

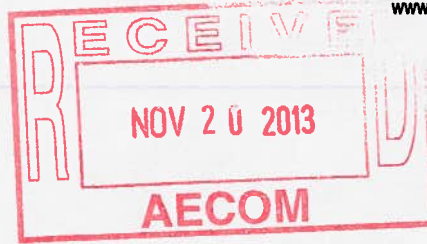
dsimpson@aldervillefirstnation.ca

Tele: (905) 352-2662

Fax: (905) 352-3242

November 8, 2013

Mr. Brian Ruck, P.Eng. C.V.S.,
Consultant Project Manager
AECOM Canada Ltd.
300 Water Street
Whitby, ON L1N 9J2



60213979

COPIES

M100 -MT
Mingona -

Dear Mr. Ruck:

**RE: Notice of Public Information Centre (PIC)
Highway 401 Widening – Highway 403/410 Interchange to the Credit River
Detail Design Group Work (G.W.P 2150-01-00 and G.W.P. 2152-01-00)
City of Mississauga**

Thank you for your letter dated October 30, 2013 informing the Board of the PIC to be held on November 13, 2013 for the above noted project. The Peel District School Board is interested in this project as we have a school in the vicinity of the project area. Please keep us informed of the status of this project and provide us with any information available so that we may monitor its progress and provide comments as necessary.

If you require any further information please contact me at 905-890-1010, ext. 2724.

Yours truly,

Branko Vidovic
Planning Assistant
Planning and Accommodation Department

c. D. Dundas, Peel District School Board
M. Zhou, Ontario Ministry of Transportation

401 Widening_hwy403_410 to credit river PIC.doc

Trustees

Janet McDougald, Chair
Suzanne Nurse, Vice-Chair
Stan Cameron
Beryl Ford
David Green
Meredith Johnson

Steve Kavanagh
Sue Lawton
Brad MacDonald
Harinder Malhi
Jeff White
Rick Williams

Director of Education and Secretary
Tony Pontes

**Associate Director,
Instructional Support Services**
Scott Moreash

**Associate Director,
Operational Support Services**
Jaspal Gill (Acting)

Meetings

Minutes of Meeting

Date of Meeting	June 17, 2011	Start Time	9:00am	Project Number	60213979
Project Name	Highway 401 Widening from Highway 403/410 Interchange to the Credit River – Detail Design G.W.P. 2150-01-00				
Location	City of Mississauga – 8th Floor, 201 City Centre Drive, Mississauga, ON L5B 2T4				
Regarding	Project Update and Opportunities for MTO/City Coordination				
Attendees	Miao Zhou, MTO Senior Project Engineer Brian Ruck, AECOM Project Manager Steve Barrett, City of Mississauga, Manager - Transportation Asset Mgmt. Abdul Shaikh, City of Mississauga, Transportation Project Engineer Farhad Shahla, City of Mississauga, Transportation Project Engineer Larry Sarris, MTO Environmental Planner Mirjana Osojnicky, AECOM Environmental Planner				
Distribution	Attendees Aimee Rose Tupaz, Corridor Management Engineer, MTO				
Minutes Prepared By	M. Osojnicky				

PLEASE NOTE: If this report does not agree with your records of the meeting, or if there are any omissions, please advise, otherwise we will assume the contents to be correct.

Schedule	Action
<ul style="list-style-type: none"> • AECOM advised that the Transportation Environmental Study Report (TESR) was filed in 2005 and Minister approved in 2007 after the resolution of two (2) bump-up requests. • Project is currently in the Pre-Design phase, scheduled for completion by the end of 2011. Detail Design will commence in 2012 and the MTO is working under a tight timeline in order to achieve planned construction in 2013. • Project includes: <ul style="list-style-type: none"> ○ Mavis Road interchange improvements ○ Transfer lanes – best fit and alternatives ○ T-Ramp connection to facilitate HOV lanes and exit at McLaughlin Road ○ 410/403/401 Ramps ○ Removal of the 2nd Line West structure as per TESR approval • There may be opportunities for the City and MTO to work together and identify issues at the beginning of the project to benefit both. 	

<ul style="list-style-type: none"> City must decide in a relatively short period of time if it plans to pursue a pedestrian bridge over Highway 401 to tie into the MTO project schedule and take advantage of construction/staging opportunities, as there may be implications for the current project schedule as well as EA approvals. 	<p>City</p>
<p><u>2nd Line West Structure</u></p>	
<ul style="list-style-type: none"> City is currently undertaking a Traffic Impact Study on Sombrero Way. It was noted that the 2nd Line West Structure is not currently being used extensively from a traffic perspective and that the increased traffic on Sombrero Way is independent of the 2nd Line West closure. 	
<ul style="list-style-type: none"> There is interest from the local Councillor as well as ratepayers for the provision of a pedestrian crossing over Highway 401. 	
<ul style="list-style-type: none"> AECOM advised that the cost to construct a pedestrian crossing over the ultimate 12-lane Highway 401 is in the range of \$4-23 million. The least cost to construct would occur at the same time as the widening of Highway 401 to take advantage of staging opportunities; future retrofitting/staging is costly. 	
<ul style="list-style-type: none"> Opportunity may exist to provide for a pedestrian/cycling crossing at Fletcher’s Creek and maintain wildlife connectivity between the north and south side of Highway 401. There remain issues that require further study with this design concept. 	
<ul style="list-style-type: none"> The existing deer crossing to the west of Fletcher’s Creek was (likely) originally a cattle crossing; low occurrence of deer currently using the structure as the property on the north side of Highway 401 is fenced off. It may be used by other animals and is also a drainage feature. 	
<ul style="list-style-type: none"> Fletcher’s Creek Crossing is still in relatively good condition however, it is nearing its life span. Replacement/rehabilitation opportunities are being considered within the current project scope. 	
<ul style="list-style-type: none"> City wants MTO to include the new pedestrian overpass in the 401 project; MTO expressed concerns for possible impact to the schedule. Both parties expressed interest in working together. 	
<ul style="list-style-type: none"> City will discuss options with Senior Staff on how it would like to proceed with respect to 2nd Line West (i.e. pedestrian crossing or multi-use underpass at Fletcher’s Creek) and advise the MTO within a week. 	<p>City</p>
<p><u>Mavis Road Interchange Improvements</u></p>	
<ul style="list-style-type: none"> City requested that the MTO ‘protect’ for a future direct connection to Belgrave Road. City understands that this connection would not be built as part of the current project. The City would have to initiate a separate Municipal Class EA to allow this road extension but requested that the MTO design not preclude these future improvements. 	
<ul style="list-style-type: none"> AECOM advised that there may be issues with the on-ramp at Mavis Road to eastbound Highway 401, as well as property impacts on the south side of Mavis Road with this design concept. 	
<ul style="list-style-type: none"> Additional studies would be required to evaluate the geometry for either an overpass/underpass to provide the Belgrave access and MTO would require 	

Senior Management approval.	
<ul style="list-style-type: none"> • City will discuss options with Senior Staff on how it would like to proceed with the potential Belgrave direct-access and advise the MTO within a week. • City suggested an at-grade crossing. MTO mentioned that Belgrave Road falls within MTO's CAH permit control. This is not a simple design exercise and MTO's Corridor Office will need to get involved in this municipal initiative. 	City
<u>Highway 403/410 Interchange</u>	
<ul style="list-style-type: none"> • AECOM is confirming the viability of the remnant ramps through modelling; they were approved in the TESR (2005). 	
<ul style="list-style-type: none"> • City shows these ramps in its long-term transportation plans and strongly recommends their construction. • City advised that the traffic modelling should account for the Light Rail Transit (LRT) initiative on Hurontario Street. 	

**Post meeting note: MTO confirmed that MTO's EMME/2 2031 includes LRT on Hurontario Street.



AECOM
 5080 Commerce Boulevard
 Mississauga, ON, Canada L4W 4P2
 www.aecom.com

905 238 0007 tel
 905 238 0038 fax

Minutes of Meeting

Date of Meeting	August 3, 2011	Start Time	9:00am	Project Number	60213979
Project Name	Highway 401 Widening from Highway 403/410 Interchange to the Credit River – Detail Design G.W.P. 2150-01-00				
Location	City of Mississauga – 8th Floor, 201 City Centre Drive, Mississauga, ON L5B 2T4				
Regarding	Project Update and Opportunities for MTO/City Coordination				
Attendees	Miao Zhou, MTO Senior Project Engineer Brian Ruck, AECOM Project Manager Steve Barrett, City of Mississauga, Manager - Transportation Asset Mgmt. Abdul Shaikh, City of Mississauga, Transportation Project Engineer Farhad Shahla, City of Mississauga, Transportation Project Engineer Al Jeraj, City of Mississauga, City Surveyor Jacquelyn Hayward Gulati, City of Mississauga, Manager Cycling Office Bob Sasaki, City of Mississauga, Manager Transportation Planning Larry Sarris, MTO Environmental Planner Clement Shim, MTO Senior Structural Engineer Mirjana Osojnicki, AECOM Environmental Planner				
Distribution	Attendees Aimee Rose Tupaz, Corridor Management Engineer, MTO Robert Fulton, Senior Surveyor, MTO				
Minutes Prepared By	M. Osojnicki				

PLEASE NOTE: If this report does not agree with your records of the meeting, or if there are any omissions, please advise, otherwise we will assume the contents to be correct.

<u>Purpose of Meeting</u>	Action
<ul style="list-style-type: none"> • Follow up to June 17, 2011 meeting and updates to: <ul style="list-style-type: none"> ○ Potential for pedestrian overpass in vicinity of 2nd Line West versus multi-use underpass at Fletcher’s Creek ○ Closure of 2nd Line West structure ○ Mavis Road Interchange improvements - Belgrave Road • Environmental Assessment (EA) Perspective and Project Schedule 	
<u>Pedestrian Access – Overpass vs. Underpass</u> <ul style="list-style-type: none"> • AECOM advised that both an overpass and underpass are feasible from a constructability point of view. • Scheduling is the key risk factor since construction for this project is planned for 	

<p>2013 and a pedestrian overpass would require either an EA Addendum to the 2007 approved Transportation Environmental Study Report (TESR) (MTO would assume proponentcy) or a separate Municipal Class EA (City would assume proponentcy).</p> <ul style="list-style-type: none"> • There are also environmental constraints/risks associated with the overpass since one of MOE's Conditions of Approval rested with the closure of the 2nd Line West structure and naturalization of the Meadowvale Station Woods area. • It was also re-emphasized that a Stormwater Management Pond was proposed in Preliminary Design to occur on the north side of Highway 401 immediately east of the Meadowvale Station Woods area and that an overpass at this location would conflict with the capacity requirements of the pond. 	
<ul style="list-style-type: none"> • There is awareness in the community for the closure of 2nd Line West structure. • City is interested in providing cyclists/pedestrians with a crossing opportunity across Highway 401 in this area. 	
<p><u>Fletcher's Creek Culvert – Multi-Use Underpass</u></p> <ul style="list-style-type: none"> • AECOM advised the existing Fletcher's Creek twin cell culvert will be replaced as part of the EA with a bridge approximately 16.5m and will accommodate a 4.5m path underneath (based on 2yr storm event for the path). • Cost-sharing would result in a difference of approximately \$2 million. • MTO provided examples and photos of similar structures at Highway 401/ Leslie where a walking/cycling trail besides the Don River exists, below the existing 401 core/collector system. • May be an opportunity to construct the proposed clear span structure over Fletcher's Creek in two parts to provide additional natural lighting and air circulation and reduce the tunnel effect, thus making it more attractive to wildlife and increased safety for persons using the underpass. The location of the core/collector transfer will determine if one or two bridges is feasible. • Meeting with MNR is taking place in August to present this idea. 	
<ul style="list-style-type: none"> • Manager of City's Cycling Office (Ms. Jacquelyn Hayward Gulati) briefly joined the meeting and provided support in principle for the underpass; it would likely be used by recreational cyclists as opposed to commuter cyclists. 	
<ul style="list-style-type: none"> • City would support a 2-bridge option from a safety perspective and will consult with Commissioner and local Councillor. • AECOM/MTO to notify City which alternative is selected (1 or 2-bridge option) 	<p>City AECOM/MTO</p>
<p><u>Pedestrian Overpass</u></p> <ul style="list-style-type: none"> • Higher risk due to scheduling: <ul style="list-style-type: none"> ○ Provincial consultation moratorium will be in effect due to November election. ○ Substantial cost increase if the City were to require EA approval but constructed at a later date because of staging (i.e. City's request to construct piers is not preferred from a staging perspective as it would be very costly to stage Highway 401 at a later time). • City to consult with Commissioner for decision in about a week. 	<p>City</p>
<p><u>Closure of 2nd Line West</u></p> <ul style="list-style-type: none"> • City advised that parts immediately north and south of Highway 401, Parts 1 and 2 on Plan 43R-20987, were closed in July 1995 by By-law 215-95, but 	

<p>several months later, in October 1995, the same two parts were re-opened by By-law 433-95; therefore they are still public highway.</p> <ul style="list-style-type: none"> • A new by-law is required for the same closure – City is to initiate and the process takes approximately 6 months to complete. • Although the City noted that there is no traffic justification for maintaining the 2nd Line West overpass, the Mississauga Official Plan does not indicate support for closure or opening of 2nd Line West. • Some ambiguity regarding which policy document supersedes: <ul style="list-style-type: none"> ○ By-law for closure (By-law 215-95) ○ By-law to re-open (By-law 433-95) ○ Approved TESR based on By-law 215-95 • City to obtain legal opinion. However, the City noted that there was an agreement between the City of Mississauga and the Ministry (shared 1982 document with MTO) and the closure of 2nd Line West was included in the 2007 TESR. • The physical closure of 2nd Line West is required prior to construction commencement. • City will take this to the Commissioner and will let MTO know soon if the City will pass a new by-law to close 2nd Line West. This issue will be discussed at the next liaison meeting, if not resolved before that. 	<p>City</p> <p>City</p> <p>City</p>
<p><u>Mavis Road Interchange Improvements – Potential Belgrave Road extension</u></p> <ul style="list-style-type: none"> • AECOM advised that (two) 2 options are being considered for the configuration of the Mavis Road interchange; one (1) of the options would provide space and flexibility to allow for a future direct access to Belgrave Road. • AECOM to notify City with selection of preferred option. 	<p>AECOM</p>
<ul style="list-style-type: none"> • A grade-separated ramp would be required to facilitate the connection. • Limited by property requirements identified in the TESR; if additional property were required, then a TESR Addendum is needed. 	
<ul style="list-style-type: none"> • City to look at conceptual design and capital budget and update AECOM/MTO on how it would like to proceed with potential direct access to Belgrave Road. 	<p>City</p>

Minutes of Meeting

Date of Meeting	March 7, 2012	Start Time	1:30pm	Project Number	60213979
Project Name	Highway 401 Widening from Highway 403/410 Interchange to the Credit River – Detail Design G.W.P. 2150-01-00				
Location	City of Mississauga – 8th Floor, 201 City Centre Drive, Mississauga, ON L5B 2T4				
Regarding	Project Updates – Belgrave Road Extension/2 nd Line West Options/Mavis Road Interchange Staging				
Attendees	Miao Zhou, MTO Senior Project Engineer Brian Ruck, AECOM Project Manager Jan Wieczorek, AECOM Highway Design Steve Barrett, City of Mississauga, Manager - Transportation Asset Mgmt. Abdul Shaikh, City of Mississauga, Transportation Project Engineer Farhad Shahla, City of Mississauga, Transportation Project Engineer Larry Sarris, MTO Environmental Planner Cameron Bevers, MTO Traffic Hossein Hosseini, MTO Project Engineer Mirjana Osojnicky, AECOM Environmental Planner				
Distribution	Attendees				
Minutes Prepared By	M. Osojnicky				

PLEASE NOTE: If this report does not agree with your records of the meeting, or if there are any omissions, please advise, otherwise we will assume the contents to be correct.

	Action
<p><u>Potential Belgrave Road Extension</u></p> <ul style="list-style-type: none"> • At the City's Request AECOM presented three (3) options for a connection to Belgrave Road from the new Mavis Road interchange: <ul style="list-style-type: none"> A. Tunnel Under Mavis Road and S-E Ramp <ul style="list-style-type: none"> ○ Access off of Highway 401 east would be split into two approaching Mavis Road. Ramp W-N/S would approach Mavis Road at-grade while a second ramp W-E would tunnel Mavis Road [and S-E Ramp] connect to Belgrave Road. ○ Requires staging work under the MTO contract otherwise cost will be higher. ○ Significant grading and staging is involved in addition to detours on Mavis Road. B. At-Grade Intersection with Mavis Road <ul style="list-style-type: none"> ○ A south-east ramp would be constructed under the Belgrave Road extension. ○ Provides natural gravity flow of water (drainage). 	

<ul style="list-style-type: none"> ○ Provides free flow of traffic and provides a right turn north to Mavis Road. ○ MTO would construct a south-east ramp on a lower profile and the City would later build a bridge over the ramp to connect Belgrave Road. ○ Some additional property may be required however same property owner (Orlando) and not in excess of what was identified in the Transportation Environmental Study Report (TESR). <p>C. At-Grade with a Full Intersection at Mavis Road</p> <ul style="list-style-type: none"> ○ Additional property would be required (more so than for Options A or B). ○ Not preferred since this is a heavy traffic volume area. <ul style="list-style-type: none"> ● Option B is preferred by both the MTO and City due to: <ul style="list-style-type: none"> ○ Operational benefits. ○ Provision of a 'right-out' northbound on Mavis. ○ City provides some investment with MTO current contract in order to protect for future improvements. <p>**City may utilize these three (3) options in its evaluation of alternatives for its Municipal Class EA for a Belgrave Road extension.</p> <ul style="list-style-type: none"> ● AECOM to provide a memo to the City summarizing pros/cons of the above options as well as a cost estimate (including confirmation of any additional property impacts). 	<p>AECOM</p>
<p><u>Staging for Mavis Road</u></p> <ul style="list-style-type: none"> ● 2nd Line West structure must be removed by the end of Stage 1 (i.e., 2014 at the earliest). ● Mavis Road structure must be extended before traffic can be shifted from core to collector lanes. This structure was designed to accommodate an expansion. ● Mavis Road will be reduced to two (2) lanes in either direction during bridge expansion for one (1) construction year (i.e., May – November). ● City asked that the curb lane be left a little wider to accommodate truck traffic. AECOM advised this should not be a problem. 	
<p><u>2nd Line West</u></p> <ul style="list-style-type: none"> ● AECOM presented several options for 2nd Line West once the existing structure is removed: ● Option A: Replace with a Vehicular Bridge and Two (2) Bike Lanes <ul style="list-style-type: none"> ○ Bridge would have to be constructed higher to accommodate vehicles and bicycles. ○ AECOM calculated cost of a new bridge to be approximately \$3.6 million dollars. ○ Option to work with current MTO schedule (i.e., take advantage of staging opportunities). ○ City would need to complete a Municipal Class EA. 	

<ul style="list-style-type: none"> ○ Issue with the location of proposed stormwater management pond; technically feasible but would have to consider other locations and methods for dealing with stormwater (i.e., possibly relocating the pond to southwest quadrant of Mavis Road interchange or other linear ponds along Highway 401). ○ Other issues with conditions of EA approval to re-vegetate this area. ● Option B: Pedestrian Bridge <ul style="list-style-type: none"> ○ Approximate cost: \$1.2 million dollars for a steel box girder structure if constructed at the same time as Highway 401 expansion. ○ Possibility to skew the alignment to allow for the construction of the stormwater management pond. ○ Option to work with MTO schedule (i.e. staging as noted above). ○ City would need to complete a Municipal Class EA. ○ This option may provide better functionality than a trail under proposed Fletcher’s Creek bridge(s) which has issues with headroom, safety and MNR permitting. ● City advised that a petition has been signed by 500 members of the local public to re-investigate the option of replacing the existing structure with a new vehicular bridge. City has an obligation to investigate the request and respond to the public. 	
<p><u>Next Steps:</u></p> <ul style="list-style-type: none"> ● City will continue to investigate the underpass (trail) option and will continue its attempts to consult with the MNR. To date, the MNR has only provided an ‘Information Gathering Form’ as per its new guidelines which came into effect February, 2012. No other communication has occurred from the City’s request to the MNR to consider a trail using the existing proposed sizes of the Fletcher’s Creek Bridge(s) rather than lengthening the structures even further. ● City will look at costs presented by AECOM for the 2nd Line West crossing of Highway 401 and will highlight areas of potential savings to senior management. ● City will make a formal request to the Ministry of Transportation to make a decision on the feasibility of constructing a new vehicular bridge crossing at 2nd Line West. ● AECOM to prepare a memo/costs for the at-grade intersection for the Belgrave Road extension. 	<p>City</p> <p>City</p> <p>City</p> <p>AECOM</p>

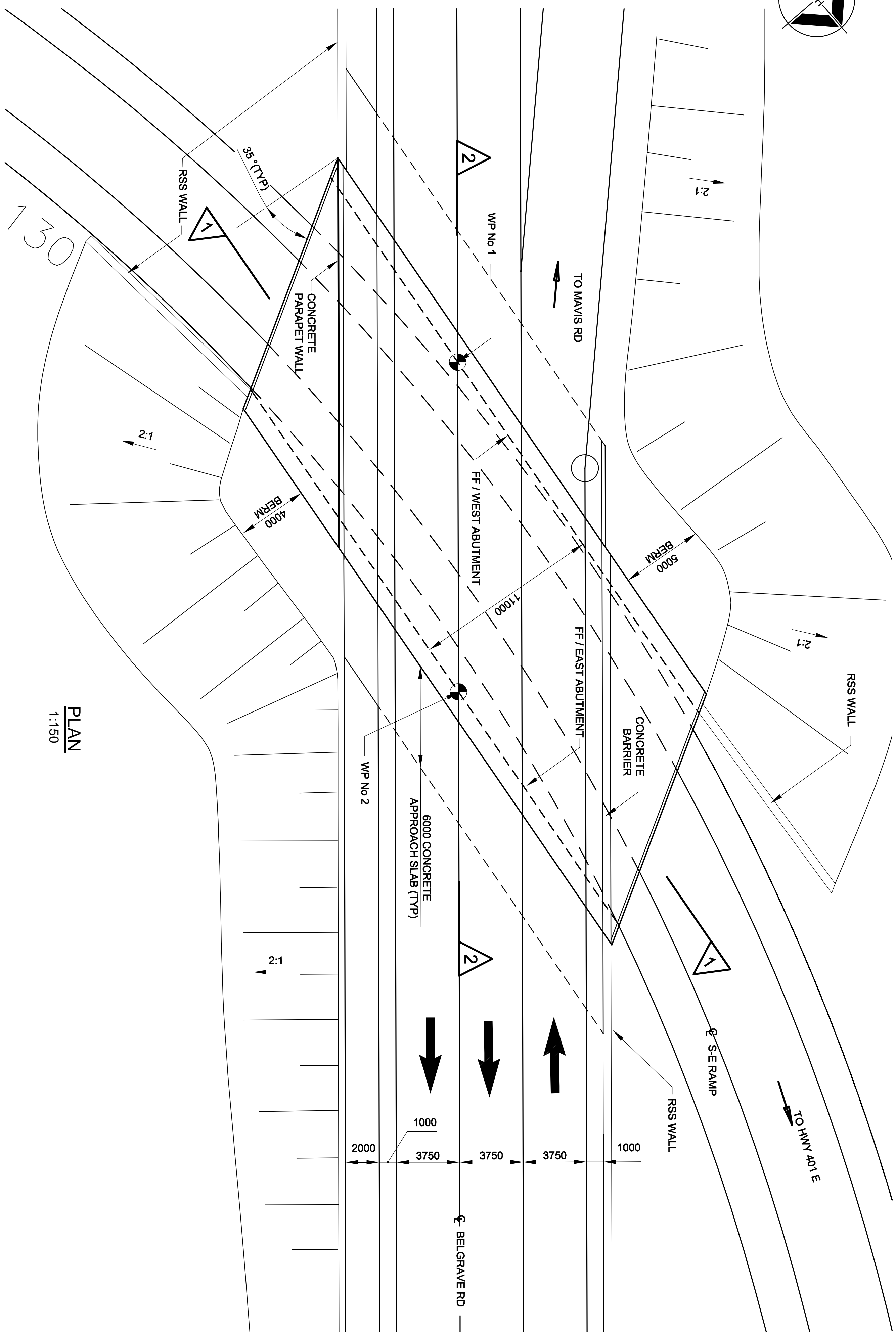
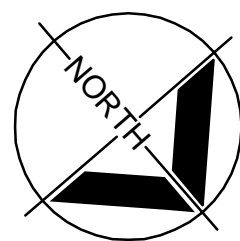
<p>of Mississauga Parks and Recreation Department on the south west side of Highway 401. Potential for joint compensation to occur.</p> <p>Mavis Road Structure</p> <ul style="list-style-type: none"> • Since no works will affect Fletchers Creek, MNR is in agreement that no permits are required. 	
--	--

3. Permitting Strategy	Action
<ul style="list-style-type: none"> • B. Ruck gave an overview of ESA Permitting Strategy. 	AECOM
<ul style="list-style-type: none"> • MNR reconfirmed that Jefferson Salamander habitat is present on the north side of Highway 401 within the MSW area. AECOM to investigate mitigation options including minimizing tree removal and examine the feasibility of a retaining wall. 	AECOM
<ul style="list-style-type: none"> • AECOM to prepare coloured drawings illustrating areas of impact for MNR to assist with ESA permit review. 	AECOM
<ul style="list-style-type: none"> • MNR discussed the ESA process and permitting requirements for Redside Dace and Jefferson Salamander. Design mitigation measures on the north side may be sufficient that Jefferson Salamander habitat may not be impacted. If this strategy is selected an ESA permit for Jefferson Salamander would not be required. 	AECOM
<ul style="list-style-type: none"> • Redside Dace construction sequencing to be further developed. 	

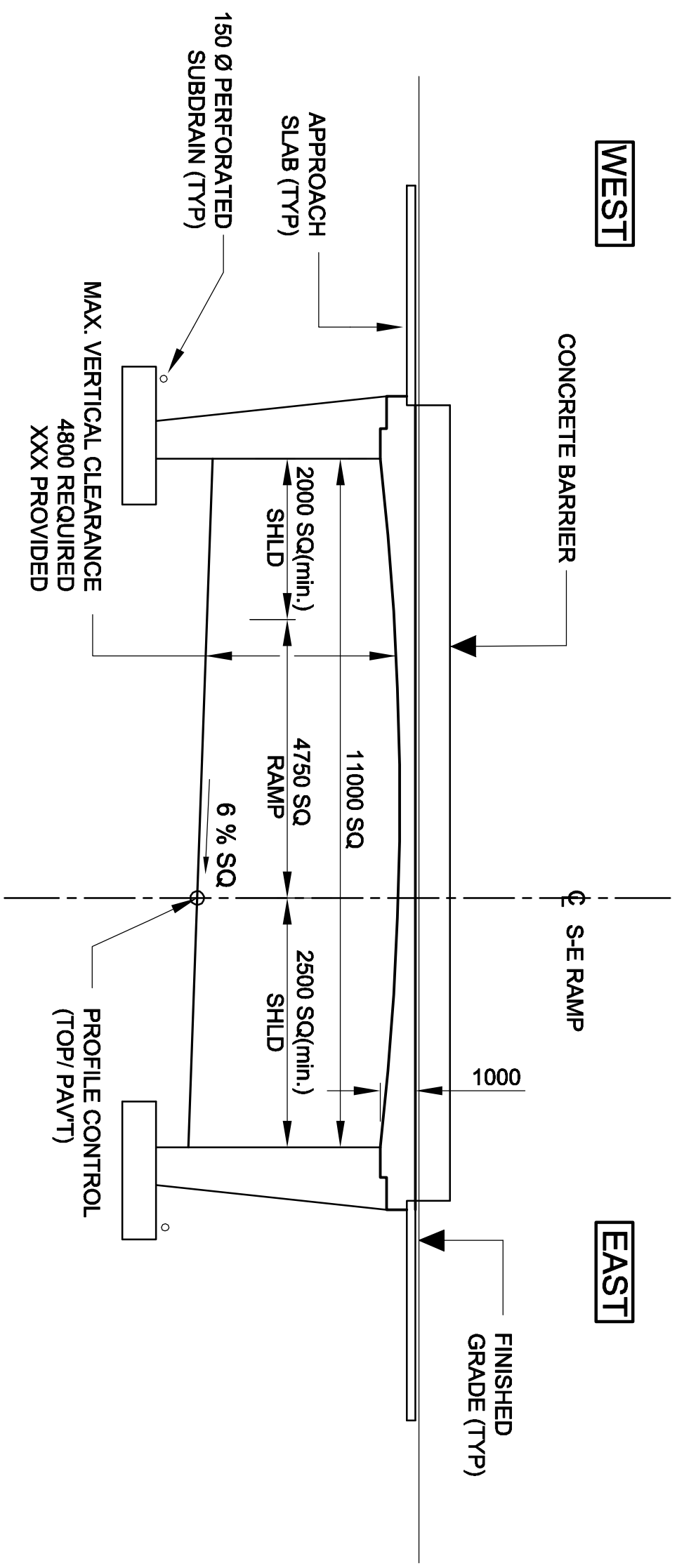
4. Next Steps	Action
<ul style="list-style-type: none"> • MNR to acknowledge acceptance of ESA Information Gathering Form (IGF). 	MNR
<ul style="list-style-type: none"> • The Alternative Avoidance Form (AAF) will then be submitted and will identify alternatives that avoid SAR regulated areas; otherwise the proponent will elect to proceed with a permit under the Act. 	
<ul style="list-style-type: none"> • MTO to determine approach – concept design for Design Build or convention approach. 	MTO
<ul style="list-style-type: none"> • District Level approved MNR ESA Permit issued to MTO required by December 2013; full permit required by summer 2014. 	AECOM/MTO
<ul style="list-style-type: none"> • Provide memo update confirming TESR conditions have been met (including restoration plans) for MOE. 	AECOM

Appendix B

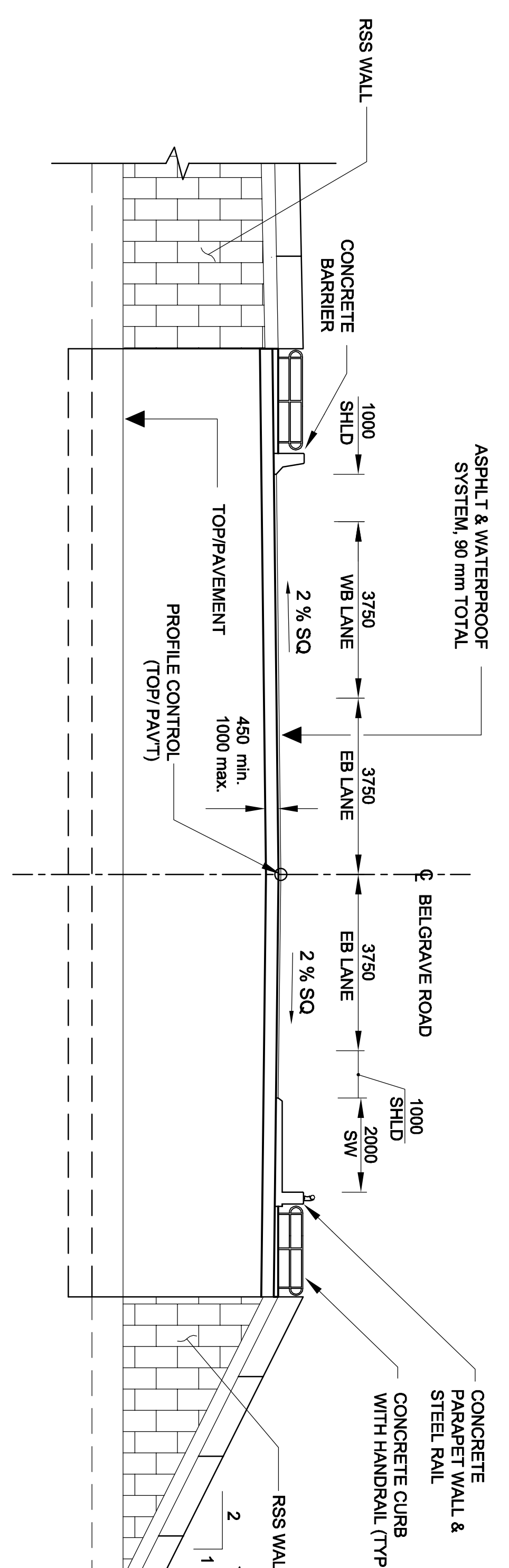
Recommended Plan



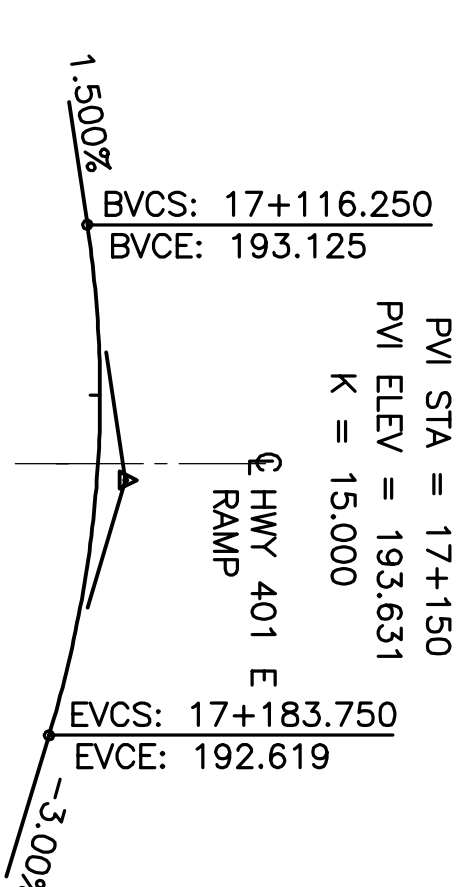
PLAN
1:150



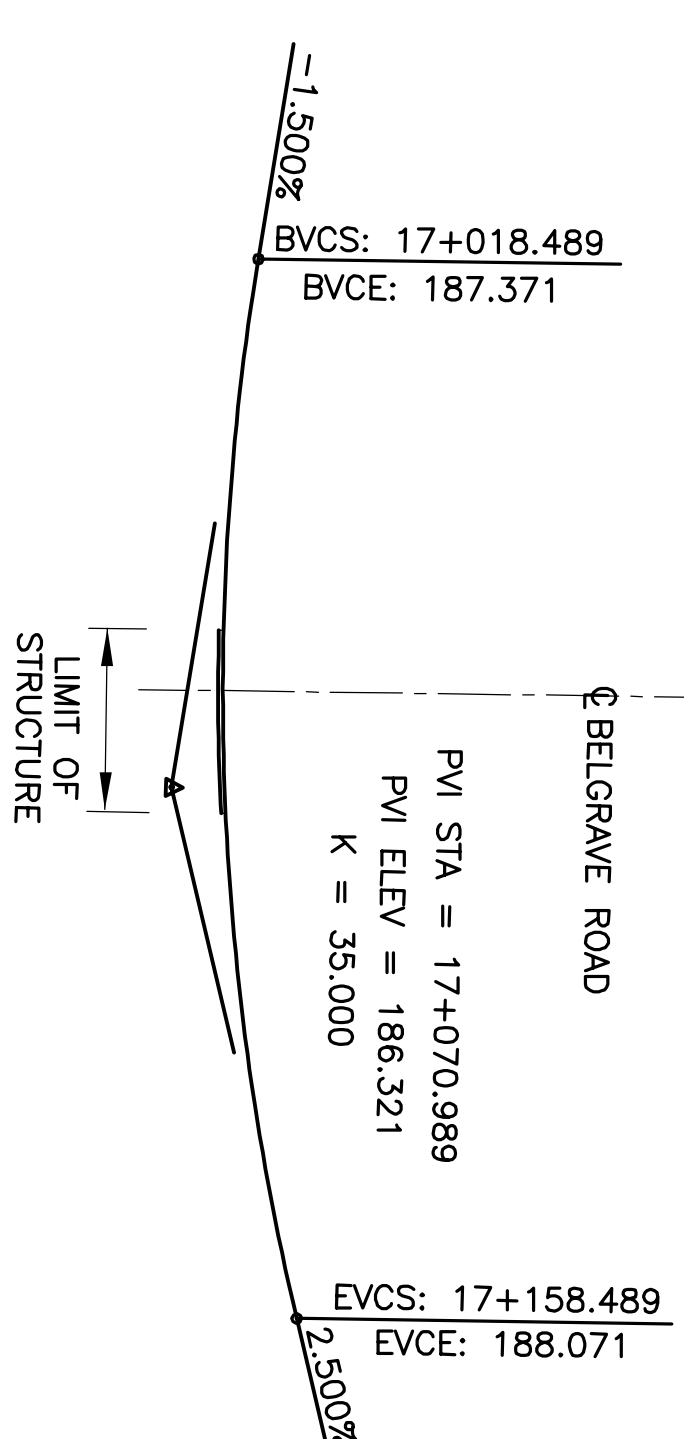
WEST
1:150



EAST
1:150



PROFILE - BELGRAVE ROAD
N.T.S



PROFILE - MAVIS ROAD
SOUTH TO HWY 407 E RAMP
N.T.S

NOTE:
ALL HORIZONTAL DIMENSIONS ARE MEASURED PERPENDICULAR TO BELGRAVE ROAD

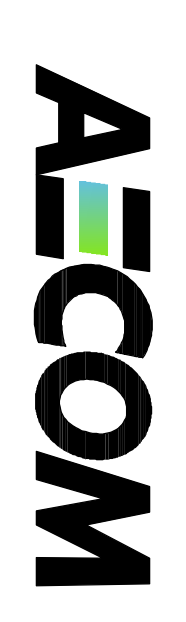
CLIENT:
CITY OF MISSISSAUGA

PROJECT:
HWY 401 WIDENING MAVIS ROAD

DRAWING:
BELGRAVE ROAD CONNECTION RECOMMENDED PLAN - ALTERNATIVE B

GENERAL ARRANGEMENT

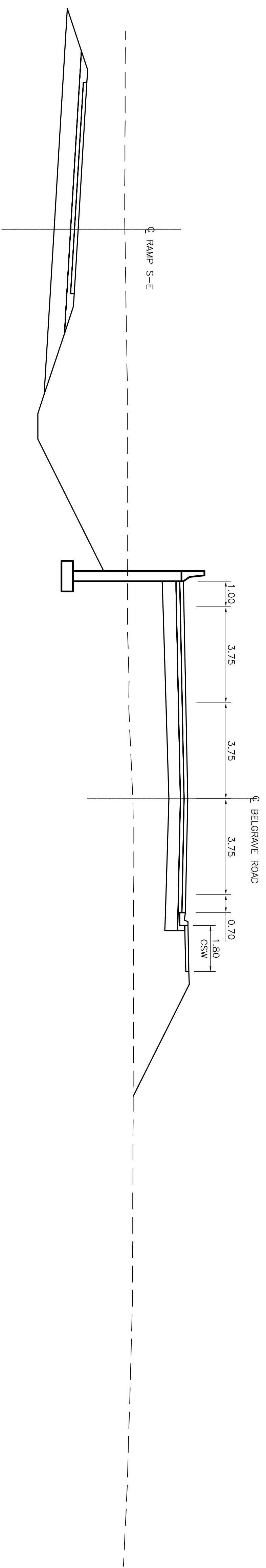
DRAWN BY:	CHECKED BY:	PROJECT NO.:
K.K.	M.S.	60213979
DESIGNED BY:	APPROVED BY:	DRAWING NO.:
SCALE:	DATE:	
1:1000	March 24, 2015	



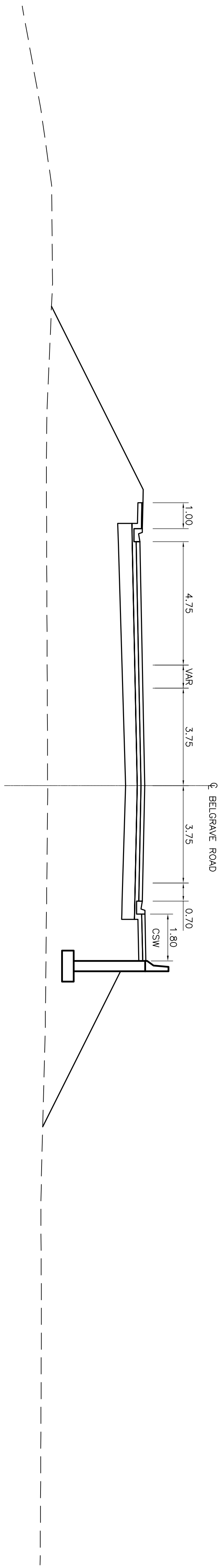
AECOM Canada, Ltd.
300 Water Street, Whistler, Ontario, Canada L1N 8J2
T905.668.9365 F905.668.0221

This drawing has been prepared for the use of AECOM's client and may not be used for any other project without the written consent of AECOM. AECOM and its employees shall not be held responsible for any errors or omissions in this drawing. All measurements shall be derived from stated dimensions. Check and label this document. All measurements shall be derived from stated dimensions.

NO.	DATE	BY	ISSUES / REVISIONS



BELGRAVE ROAD
EAST OF RAMP S-E



BELGRAVE ROAD
WEST OF RAMP S-E

HORIZONTAL SCALE = 1:100
0 1m 5m
VERTICAL SCALE = 1:100

NO.	DATE	BY	ISSUES / REVISIONS



AECOM Canada Ltd.
300 Water Street, Whiteby, Ontario, Canada L1N 3J2
T905.668.9383 F905.668.0221

This drawing has been prepared for the use of AECOM's client and policy holders and is not to be used for any other purpose without the written consent of AECOM. AECOM makes no warranty, expressed or implied, in any way that it will be suitable for any other purpose. Do not make the document. All measurements must be obtained from stated dimensions.

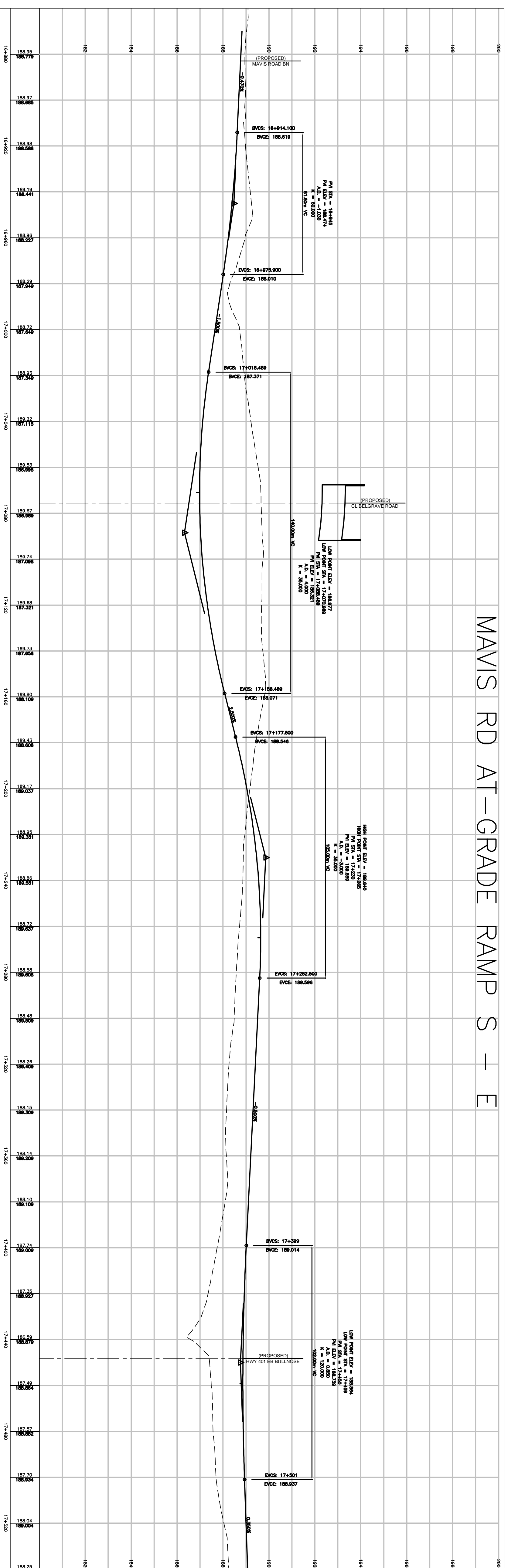
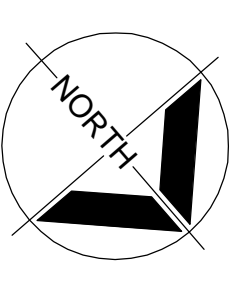
CLIENT:

PROJECT:
HWY 401 WIDENING
MAVIS ROAD

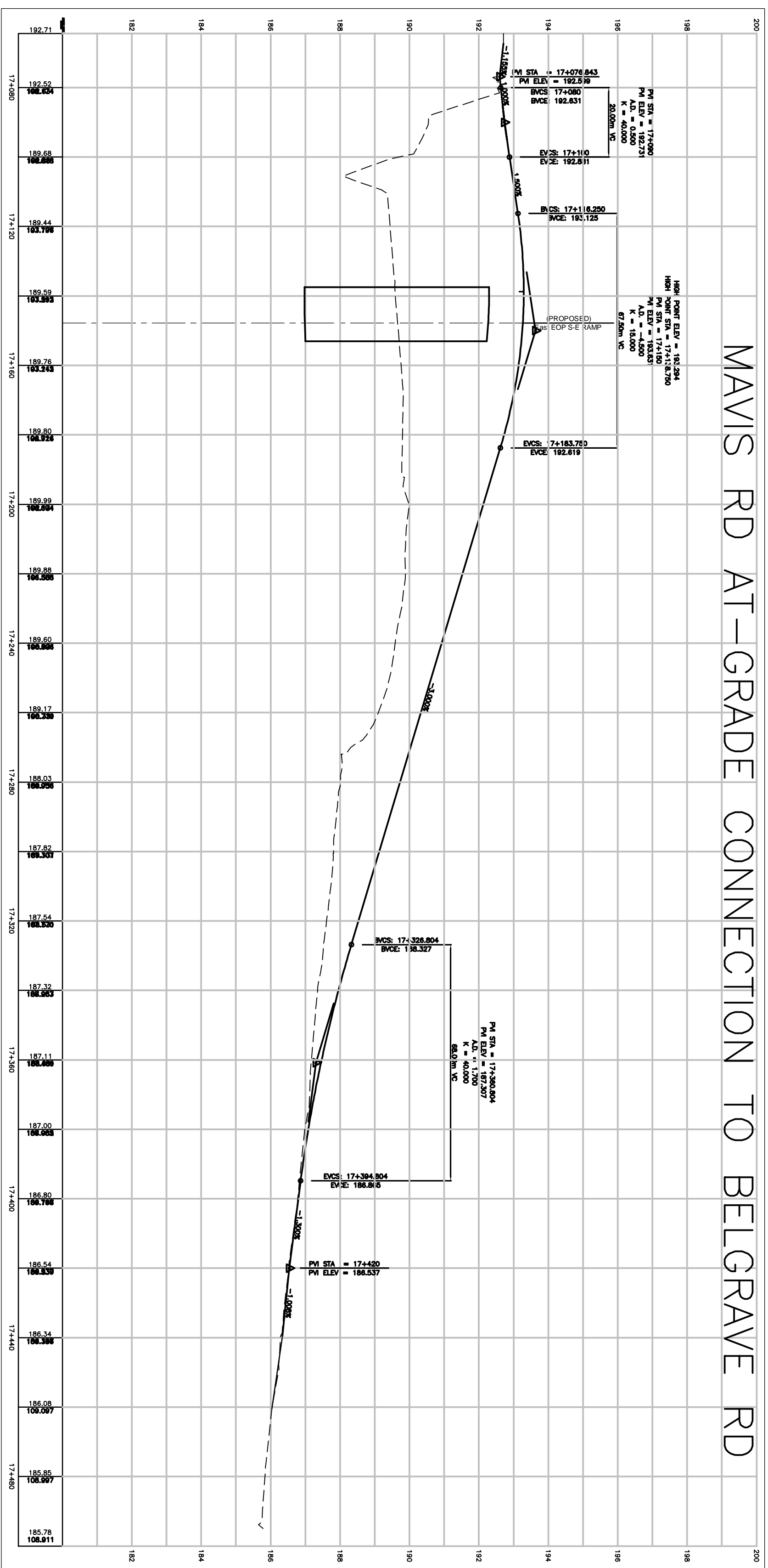
DRAWN BY:
BELGRAVE ROAD CONNECTION
RECOMMENDED PLAN - ALTERNATIVE B
AT-GRADE

DRAWN BY:	CHECKED BY:	PROJECT No.:
R. AUGER		60213979
DESIGNED BY:	APPROVED BY:	DRAWING No.:

SCALE:	DATE:
H=1:100 V=1:100	March 25, 2015



MAVIS RD AT-GRADE CONNECTION TO BELGRAVE RD



HORIZONTAL SCALE = 1:1000
 0 10m 50m
 VERTICAL SCALE = 1:100
 0 5m

0 10m 50m 100m

NO.	DATE	BY	ISSUES / REVISIONS

AECOM
 AECOM Canada, Ltd.
 300 Water Street, Whittby, Ontario, Canada L1N 8J2
 T905.668.8985 F905.668.0221

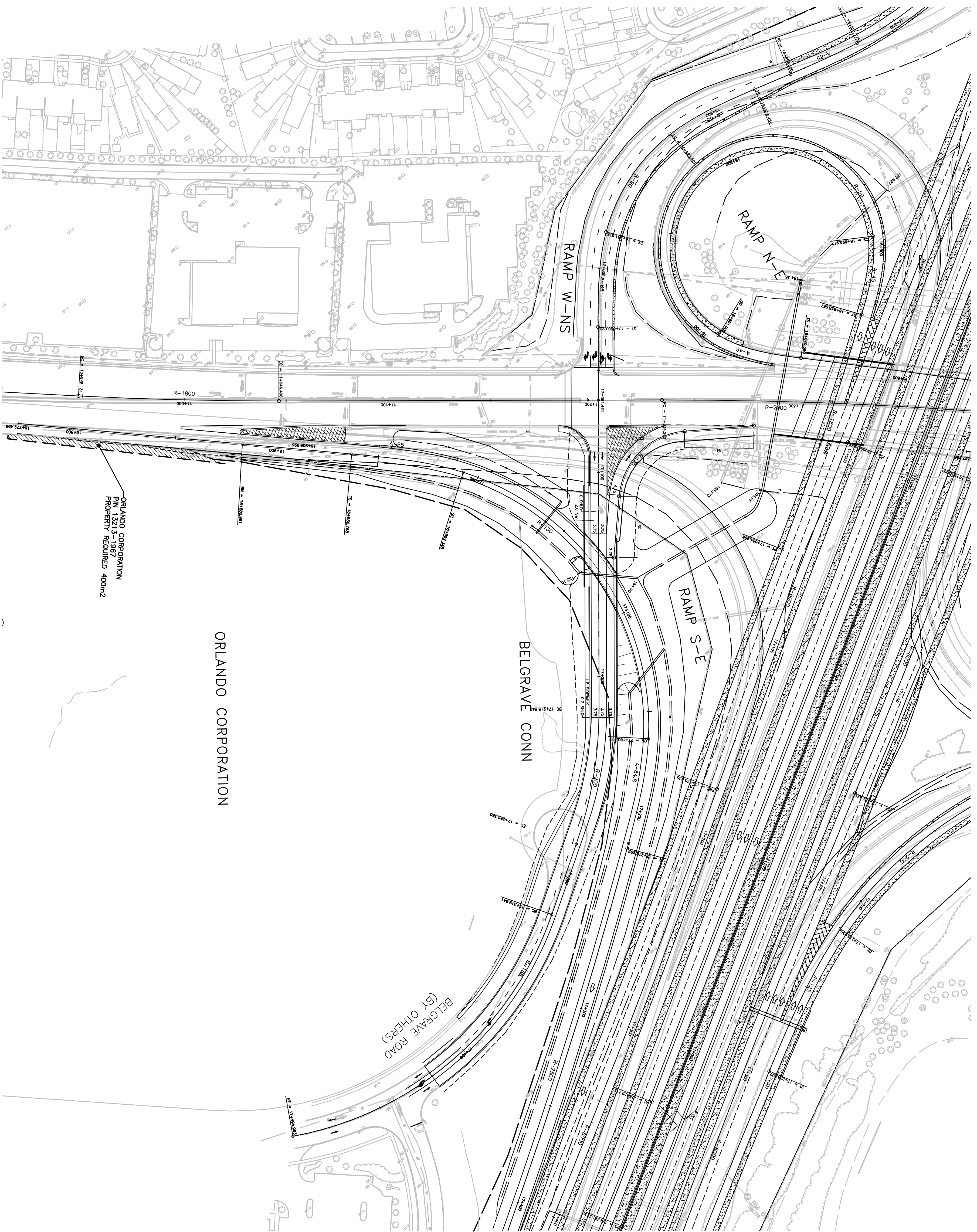
CLIENT:
 PROJECT:
**HWY 401 WIDENING
 MAVIS ROAD**

PROJECT:
**HWY 401 WIDENING
 MAVIS ROAD**

DESIGNED BY: R. ALGER
 CHECKED BY:
 PROJECT NO.: 60213979

SCALE: H=1:1000 V=1:100
 DATE: March 25, 2015

DRIVING BELGRAVE ROAD CONNECTION
 RECOMMENDED PLAN - ALTERNATIVE B
 AT-GRADE

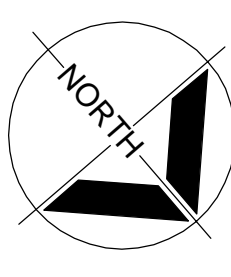


ORLANDO CORPORATION

ORLANDO CORPORATION
PIN 13213-1987
FRONTIERY REQUIRED 400m2

HORIZONTAL SCALE = 1:1000
0 10m 50m
0 2m 10m
VERTICAL SCALE = 1:200

0 10m 50m 100m



No.	DATE	BY	ISSUES / REVISIONS

AECOM
AECOM Canada, Ltd.
300 Water Street, Whistler, Ontario, Canada L7N 8J2
T905.668.8363 F905.668.0221

This drawing has been prepared for the use of AECOM's client and may not be used for any other purpose without the written consent of AECOM. AECOM and the AECOM logo are registered trademarks of AECOM. All other trademarks are the property of their respective owners. Do not scale the document. All measurements must be obtained from stated dimensions.

CLIENT: _____

PROJECT:
**HWY 401 WIDENING
MAVIS ROAD**

DRAWING:
**BELGRAVE ROAD CONNECTION
RECOMMENDED PLAN - ALTERNATIVE B
AT-GRADE**

DRAWN BY: R. AUGER	CHECKED BY:	PROJECT NO.:
DESIGNED BY: R. AUGER	APPROVED BY:	60213979
SCALE: 1:1000	DATE: March 25, 2015	DRAWING NO.: